

APPLICATION NO	PA/2021/2240
APPLICANT	Mr Mark Stafford, Cleveland Build Ltd
DEVELOPMENT	Planning permission to demolish 22 West Street and erect three three-storey terraced houses
LOCATION	22 West Street, West Butterwick, DN17 3LA
PARISH	West Butterwick
WARD	Axholme South
CASE OFFICER	Scott Jackson
SUMMARY RECOMMENDATION	Grant permission subject to conditions
REASONS FOR REFERENCE TO COMMITTEE	Member 'call in' (Cllr Rose due to significant public interest)

POLICIES

National Planning Policy Framework:

Paragraph 7 states, 'The purpose of the planning system is to contribute to the achievement of sustainable development. At a very high level, the objective of sustainable development can be summarised as meeting the needs of the present without compromising the ability of future generations to meet their own needs.'

Paragraph 8 states, 'Achieving sustainable development means that the planning system has three overarching objectives, which are interdependent and need to be pursued in mutually supportive ways (so that opportunities can be taken to secure net gains across each of the different objectives):

- (a) an economic objective – to help build a strong, responsive and competitive economy, by ensuring that sufficient land of the right types is available in the right places and at the right time to support growth, innovation and improved productivity; and by identifying and coordinating the provision of infrastructure;
- (b) a social objective – to support strong, vibrant and healthy communities, by ensuring that a sufficient number and range of homes can be provided to meet the needs of present and future generations; and by fostering a well-designed and safe built environment, with accessible services and open spaces that reflect current and future needs and support communities' health, social and cultural wellbeing; and
- (c) an environmental objective – to contribute to protecting and enhancing our natural, built and historic environment; including making effective use of land, helping to improve biodiversity, using natural resources prudently, minimising waste and pollution, and mitigating and adapting to climate change, including moving to a low carbon economy.'

Paragraph 9 states, 'Planning policies and decisions should play an active role in guiding development towards sustainable solutions, but in doing so should take local circumstances into account, to reflect the character, needs and opportunities of each area.' It also explains that the three overarching objectives are not criteria against which every decision can or should be judged.

Paragraph 10 states that at the heart of the Framework is a presumption in favour of sustainable development.

Paragraph 11 sets out the presumption in favour of sustainable development and confirms that, for decision-taking, this means:

- (c) approving development proposals that accord with an up-to-date development plan without delay; or
- (d) where there are no relevant development plan policies, or the policies which are most important for determining the application are out-of-date, granting planning permission unless:
 - (i) the application of policies in this Framework that protect areas or assets of particular importance provides a clear reason for refusing the development proposed; or
 - (ii) any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in the Framework as a whole.

Paragraph 38 states, 'Local planning authorities should approach decisions on proposed development in a positive and creative way...Decision-makers at every level should seek to approve applications for sustainable development where possible.'

Paragraph 47 makes clear that 'Planning law requires that applications for planning permission be determined in accordance with the development plan, unless material considerations indicate otherwise. Decisions on applications should be made as quickly as possible, and within statutory timescales unless a longer period has been agreed by the applicant in writing.'

Paragraph 130 states, 'Planning policies and decisions should ensure that developments:

- (a) will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development;
- (b) are visually attractive as a result of good architecture, layout and appropriate and effective landscaping;
- (c) are sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities);
- (d) establish or maintain a strong sense of place, using the arrangement of streets, spaces, building types and materials to create attractive, welcoming and distinctive places to live, work and visit;

- (e) optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development (including green and other public space) and support local facilities and transport networks; and
- (f) create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users; and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.

Paragraph 159 – Inappropriate development in areas at risk of flooding should be avoided by directing development away from areas at highest risk (whether existing or future). Where development is necessary in such areas, the development should be made safe for its lifetime without increasing flood risk elsewhere.

Paragraph 162 – The aim of the sequential test is to steer new development to areas with the lowest risk of flooding from any source. Development should not be allocated or permitted if there are reasonably available sites appropriate for the proposed development in areas with a lower risk of flooding. The strategic flood risk assessment will provide the basis for applying this test. The sequential approach should be used in areas known to be at risk now or in the future from any form of flooding.

North Lincolnshire Local Plan: DS1, DS7, DS14, DS16, H5, LC12, T2 and T19

North Lincolnshire Core Strategy: CS1, CS2, CS3, CS5, CS7, CS8, CS17 and CS19

Housing and Employment Land Allocations Development Plan Document (DPD)

CONSULTATIONS

Highways: No objection, but recommend conditions.

Ecology: No evidence of bat roosts was recorded. Planning conditions are proposed to minimise harm to protected and priority species and habitats and to seek a net gain in biodiversity in accordance with policy CS17, the National Planning Policy Framework and the Small Sites Metric.

LLFA Drainage: Given the sensitivity of the site and the poor draining characteristics of the subsoils, recommend that the surface water drainage system is designed to cater for the 100 year plus 40% climate change critical flood event. In addition, ask that all Source Control SuDS techniques that can store and allow water reuse are fully explored. Recommend conditions to prevent water flowing from the site onto the highway and vice versa.

Environmental Protection: Recommend a phase 1 contaminated land assessment is provided prior to the application being determined; alternatively, a full contaminated land condition should be attached to any planning permission granted. A construction hours condition is also recommended.

Tree Officer: Agrees with comments in the arboricultural report relating to the conifer trees/hedge to the north side of the site.

Regarding the lime tree within the highway and the proposed development works in proximity to the tree, the suggestion of no-dig construction within the root protection areas

of the tree is noted, and a method statement showing the construction of this would be useful. Suggests this be requested to ensure that the driveways will not damage any roots within these areas. If the council is then minded to grant consent for the development, then conditions requiring any agreed method statement to be adhered to could be included.

Archaeology: The proposal does not adversely affect any heritage assets or their settings. No further recommendations.

Environment Agency: Having reviewed the updated Flood Risk Assessment (FRA), the previous objection is withdrawn and a condition recommended requiring the development to take place in accordance with the mitigation measures set out in the FRA.

Severn Trent Water: Reminds the council of Building Regulations requirements and that foul sewerage connections can be dealt with under the Water Industries Act 1991. Recommends an informative.

Public Rights of Way: 'In summary, I still think that, on the basis of my own interpretation of this body of evidence, The Fleet lies west of 22 West Street and is therefore unaffected by the application. There is correspondence on file from Lindsey CC's County Surveyor stating that The Fleet was within the highway, but this matter was settled post Lindsey County Council, and I can see no further objection about The Fleet being part of the highway. If Highways still thought it was, however, they could still apply to have it deregistered for that reason. But that of course is not, or so I would assume, of relevance to you and the application.'

PARISH COUNCIL

Notes that there are three houses in this development. Access to the present property is across land known as The Fleet, which is common land. West Butterwick Parish Council are custodians of this land on behalf of the Parish of West Butterwick. At the present time access is only granted for one property at that site and the developer would need to enter into negotiations with the Parish Council for any further properties on that site to have access across The Fleet.

PUBLICITY

A site notice has been displayed. Four letters of objection have been received which can be summarised as follows:

- The site plan uses 5 metres of our garden. No notice has been served. The application should not have been validated. We will seek judicial review if this is not addressed.
- The proposal is out of keeping with the village due to three, three-storey dwellings replacing one modest dwelling next to a bungalow, which is over-development.
- Impact on local services
- The frontage is car parking dominated, across the village green space, part of which will be lost. Surface water from the car parking may flood the road or The Fleet.
- The Fleet should be shown on the drawings.
- Proposed side windows overlook neighbouring properties.

- Loss of a character dwelling to poorly designed dwellings. The history of the village will be lost if this is repeated elsewhere.
- There is no evidence to justify that the existing dwelling is in a very poor condition.
- An arboricultural assessment is needed for the front and rear trees.
- Twelve or more people may occupy the site and there are six parking spaces. The additional noise from these people and cars is too much.
- The size and positioning of the properties
- The proposed demolition works may harm planting on neighbouring land that is attached to the wall to be demolished. Any damage should be replaced without cost to neighbours.
- Increase in parking and congestion
- Overlooking from ground-floor windows
- Clarification sought over the proposed boundary treatment.

ASSESSMENT

This application was deferred at a previous meeting of the planning committee to allow members to visit the site before making a decision.

Site characteristics

The site consists of a detached dwelling and its garden. The site is within the defined settlement boundary of West Butterwick and within flood zone 2/3a fluvial in accordance with the SFRA 2021. There is a large highway tree to the site frontage and a mature hedge line along the rear boundary to the north. The dwelling is constructed from a mix of brick and render and is located centrally within the site. There is a low brick wall with vehicular access along the site frontage with West Street.

Proposal

Planning permission is sought to demolish the existing dwelling and erect three, three-storey town houses. Six parking spaces are proposed. Two of these are provided from the existing vehicular access. The remaining four are provided in a bank across the frontage of the new dwellings and would be accessed via a dropped kerb and crossover across the wide highway verge.

The following considerations are relevant to this proposal:

- **the principle of development**
- **impact upon the character of the area**
- **impact upon residential amenity**
- **impact upon highway safety**

- **flood risk.**

Principle of development

Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires planning applications to be determined in accordance with the development plan unless material considerations indicate otherwise. In this instance the development plan consists of the saved policies of the North Lincolnshire Local Plan (NLLP), the North Lincolnshire Core Strategy (NLCS) and the Housing and Employment Land Allocations Development Plan Document (HELADPD). Material considerations exist in the form of national policy and guidance contained within the National Planning Policy Framework (NPPF) and the suite of documents comprising National Planning Practice Guidance (NPPG).

This proposal is for the redevelopment of the site (following demolition of the existing dwelling) and the erection of three dwellings arranged in a terrace. The parts of the application site where the dwellings are proposed are located within the defined development boundary for West Butterwick as shown in the Housing and Employment Land DPD. Furthermore, the application site is in a sustainable location, within walking distance of local services, and fronts North Street, which is a local bus route. The proposal therefore accords with the principles of sustainable development as set out within the policies of the local plan, Core Strategy and the National Planning Policy Framework on delivering residential development in appropriate locations.

Policy H8 of the local plan (Housing Design and Housing Mix) applies and states that new residential development will be permitted provided that it incorporates a high standard of layout which maintains, and where possible improves and enhances, the character of the area and protects existing and natural and built features, landmarks or views that contribute to the amenity of the area. This site constitutes developable land within the settlement boundary of West Butterwick, in a sustainable location. There is, therefore, a presumption in favour of residential development.

Character of the area

The plans show the demolition of the existing detached dwelling and for it to be replaced with a terrace of three houses with parking spaces to the front and garden space to the rear. The plans show the dwellings will be located in the centre of the site, set back from the highway to allow the provision of car parking spaces to the front, and set in from the eastern/western boundaries to allow for areas of pedestrian circulation. The dwellings are shown to be sited approximately 11 metres back from the highway footpath; this ensures they are sited a similar distance from the highway as neighbouring properties to the west of the site (24 and 26 West Street).

The existing settlement pattern in this part of West Butterwick primarily consists of detached and semi-detached dwellings set within spacious plots with gardens to the rear. To the north of the site is a new housing estate (Christopher Meadows) which contains modern dwellings, single and two storeys in height, and a house within close proximity of the site (28 West Street) has been renovated and extended so it resembles a large, modern, detached dwelling in appearance and scale. Therefore, the introduction of three modern dwellings is not considered to be at odds with the character and appearance of the street scene. The development proposals will introduce built form into the street scene which is equivalent to three storeys in height (9.5 metres); however, given the mix, height and varying ages of existing housing stock (including modern dwellings) in this part of West

Butterwick, the introduction of three, three-bedroomed dwellings in this residential location is not considered to be at odds with the character and appearance of the street scene.

The dwellings are designed with a gable roof, with dormer windows to the front roof slope and rooflights to the rear. The principal elevations of the dwelling are broken up by a porch access which protrudes forward at ground-floor level and the first-floor openings are in vertical alignment with the ground-floor window and door openings. The rear elevation is broken up by a single-storey protruding element which provides a sun lounge to each property and the side elevations are broken up by a number of window openings to all floors. Details of the proposed external materials of construction have been provided on the supporting drawings, which state the dwellings will be constructed from a Wienerberger Redstone Rainbow brick (off-red mixed colour) and a Sandtoft Terracotta Neo-Pantile roof tile; these materials are considered to be consistent with the character and appearance of the street scene.

Policy LC12 of the local plan states, 'Proposals for all new development will, wherever possible ensure the retention of trees, woodland and hedgerows.'

The plans show the retention of the highway tree to the front of the site. This tree, along with a number of trees on the northern side of West Street, form an important green corridor in the highway verge and contribute to the character and appearance of the street scene. The retention of the tree is considered important in visual amenity terms and the arboricultural report confirms sufficient protection of the tree from development during the construction period. To safeguard the tree during and after the construction period, conditions are recommended requiring the development to take place in accordance with the tree protection measures outlined in the arboricultural report, no-dig driveway measures to be employed within the vicinity of the tree, and a method statement for the construction of the driveway within the root protection zone of the highway tree to be submitted for consideration. No objection has been received from the tree officer in respect of the tree protection measures set out in the arboricultural report submitted with the planning application. The plans show the mature hedge to the rear boundary of the site (the northern boundary) will be retained and afforded sufficient tree protection measures during the construction period.

Residential amenity

The plans show the dwellings will be sited in terrace form towards the centre of the site with separation distances of 8.07 metres to 24 West Street (to the west) and 14.06 metres to 20 West Street (to the east). These separation distances are considered sufficient to mitigate any potential loss of amenity to neighbouring properties through the effects of overshadowing or having an overbearing impact (taking into account the height of the dwellings). Each dwelling is shown to have two off-street parking spaces and areas of private amenity space of 188 square metres, 84 square metres and 87 square metres respectively. This is considered sufficient to meet the needs of future occupants of the dwellings, and given the separation from the side boundaries and the setback position from the public highway, it is not considered to represent over-development of the site.

The windows proposed in the side elevations of the dwellings at first and second floor, which face east and west, have the potential to overlook existing residential properties to either side of the site. However, the plans show the windows on the second floor, which serve a bedroom, will be escape windows (to meet building regulations) and will be fitted with obscure glazing to Pilkington Obscuration Level 4; and a condition is recommended

requiring the windows in the first floor, which serve a bathroom, to be fitted with Obscuration Level 3 glazing. This will ensure there is no overlooking of neighbouring properties or their gardens.

The plans clearly show the proposed method of boundary treatment to be a 1.8 metre high closed-boarded fence to all the rear boundaries. This is considered sufficient in terms of providing privacy to the proposed rear gardens and to ensure there is a secure and fixed form of boundary treatment to serve the development. In conclusion, taking into account the separation distances to existing properties, it is considered the proposed development will not result in loss of residential amenity.

Impact upon highway safety

Policy T2 of the North Lincolnshire Local Plan states that all development should be served by a satisfactory access. Policy T19 is concerned with parking provision as well as general highway safety.

The council's highways department has raised no objection to the proposed access and parking details, but recommends a series of conditions which will be attached should permission be granted. Sufficient on-site parking is provided and the accesses are considered safe. As such, there is considered to be no conflict with the requirements of policies T2 and T19.

Off-street parking spaces are proposed to the front and side of the dwellings. However, the plans show the provision of two parking spaces per dwelling. This draws comparisons with the layout of neighbouring dwellings where driveways to the front and side provide off-street parking. Therefore, the proposals are not considered to be a car dominated development.

Flood risk

The site is within Flood Zone 2/3a of the North Lincolnshire Strategic Flood Risk Assessment (SFRA) 2022. The proposal is for 'more vulnerable' development, and the sequential and exceptions tests need to be applied and passed for the proposed dwelling. A revised flood risk assessment (FRA) has been submitted with the planning application and the Environment Agency has considered the content of the revised Flood Risk Assessment and removed its previous objection; it recommends a condition requiring the development to take place in accordance with the flood mitigation measures set out in the submitted FRA.

The proposed development needs to be assessed against the sequential and exceptions tests. The entire settlement of West Butterwick is at the same level of flood risk; there are no sites available which could reasonably accommodate the development that are at a lower risk of flooding (i.e. Flood Zone 1). The sequential test is therefore passed in this case. The exceptions test then needs to be applied.

The applicant has provided additional information in the FRA to set out how the exceptions test is considered to be met for the proposed development. This supporting information sets out how the development is considered to be sustainable in terms of proximity to local services and transport routes. In terms of community benefits, the supporting statement highlights the fact that the proposal will provide additional housing, re-use a brownfield site, provide jobs for local tradesmen (during the construction period), and support local businesses and services. To pass the exceptions test a development must show that it will provide wider sustainability benefits to the community that outweigh flood risk, and that it

will be safe for its lifetime, without increasing flood risk elsewhere, and where possible reducing flood risk overall.

It is considered that the FRA satisfactorily demonstrates that the development will be safe without increasing flood risk elsewhere. It considers mitigation measures in respect of floor levels, emergency access and egress, flood warning and evacuation, and flood resilience. It is considered that sufficient justification has been submitted within the FRA to demonstrate that the proposal will deliver sustainable development in West Butterwick, be of wider community benefit in that it will support existing services within the defined rural settlement, and make a modest contribution towards housing delivery targets. Therefore, the proposal is considered to provide wider sustainability benefits which outweigh flood risk. Subsequently, the exceptions test is passed in this case and the proposal is considered to comply with policies DS16 of the North Lincolnshire Local Plan, CS19 of the Core Strategy and the National Planning Policy Framework in terms of flood risk.

Other issues

Following consultation with the council's Principal Access and Commons Officer, it is considered the land to the front of the site is not common land, despite the parish council's comments to the contrary. The Fleet common land is considered to commence some distance to the west of the site frontage and would have no bearing on the proposal. Notwithstanding this, The Fleet common land is not designated as part of the development plan and is instead protected by legal covenant. Any planning permission granted that did affect this common land would not prejudice or override any legal covenant and this would be a private legal matter separate to planning.

An issue has been raised by one of the neighbouring properties that the boundary line is incorrectly plotted along the western side of the site and the development encroaches or removes part of their land/boundary. This has been clarified with the applicant and they have confirmed the land shown within the red-edge site location plan is within their ownership and that development will not encroach into the neighbouring land. On this basis the local planning authority is satisfied that the development proposals will not encroach onto neighbouring land and there is no requirement for the applicant to serve formal notice on the landowner (by serving Certificate B).

The principle of development and associated flood risks are acceptable in accordance with policies CS1, CS2 and CS19 of the Core Strategy, policy DS16 of the local plan, and the provisions of the NPPF.

Conclusion

The proposal entails redevelopment of a brownfield infill site within the development limit of West Butterwick which is acceptable in principle. Flood risk sequential and exceptions tests are passed and the site-specific flood risk assessment is considered to be appropriate. The impact upon the character of the area and residential amenity are acceptable. The proposal is not considered to have any detrimental impacts upon highway or pedestrian safety, and sufficient protection is afforded to existing trees and hedges during development, which will be retained to contribute to the visual amenity of the area. The application is therefore recommended for approval.

RECOMMENDATION **Grant permission subject to the following conditions:**

1.

The development must be begun before the expiration of three years from the date of this permission.

Reason

To comply with section 91 of the Town and Country Planning Act 1990.

2.

The development hereby permitted shall be carried out in accordance with the following approved plans:

- 1078-21 1 of 4
- 1078-21 2 of 4
- 1078-21 3 of 4 (9/9/2022)
- 1078-21 4 of 4 (8/2/2022)
- Plan 2 (Tree Protection Plan).

Reason

For the avoidance of doubt and in the interests of proper planning.

3.

The existing vehicular access to the site shall be improved within highway limits in accordance with details to be submitted to and approved in writing by the local planning authority.

Reason

In the interests of highway safety and to comply with policy T19 of the North Lincolnshire Local Plan.

4.

No loose material shall be placed on any driveway or parking area within 10 metres of the adopted highway unless measures are taken in accordance with details to be submitted to and approved in writing by the local planning authority to prevent the material from spilling onto the highway. Once agreed and implemented these measures shall be retained.

Reason

In the interests of highway safety and to comply with policy T19 of the North Lincolnshire Local Plan.

5.

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that order with or without modification), nothing shall at any time be erected, retained, planted or allowed to grow over 1.05 metres in height above the level of the adjoining carriageway for a distance of 2 metres from the highway boundary across the site frontage.

Reason

In the interests of highway safety and to comply with policies T2 and T19 of the North Lincolnshire Local Plan.

6.

No dwelling on the site shall be occupied until the vehicular access to it and the vehicle parking spaces serving it have been completed and, once provided, the vehicle parking spaces shall be retained.

Reason

In the interests of highway safety and to comply with policies T2 and T19 of the North Lincolnshire Local Plan.

7.

No above-ground works shall take place until a scheme for the disposal of surface water has been agreed in writing by the local planning authority and none of the dwellings shall be occupied until it is connected to the approved drainage system. The surface water drainage scheme shall be designed to cater for 100 year plus 40% climate change critical flood event and shall explore all source Control SuDS techniques unless it can be demonstrated such techniques are not feasible on the site. The development shall take place in accordance with the agreed details and timings and no dwelling shall be occupied until it is connected to the agreed drainage system.

Reason

To ensure satisfactory drainage is provided in accordance with policy DS14 of the North Lincolnshire Local Plan.

8.

No above-ground works shall take place until details showing an effective method of preventing surface water run-off from hard paved areas within the site onto the highway have been approved in writing by the local planning authority. These facilities shall be implemented prior to the access and parking facilities being brought into use and thereafter so retained.

Reason

In the interests of highway safety and to comply with policy T19 of the North Lincolnshire Local Plan, policies CS18 and CS19 of the North Lincolnshire Core Strategy, and paragraphs 159 to 169 of the National Planning Policy Framework.

9.

No above-ground works shall take place until details showing an effective method of preventing surface water run-off from the highway onto the developed site have been submitted to and approved in writing by the local planning authority. These facilities shall be implemented prior to the access and parking facilities being brought into use and thereafter so retained.

Reason

In the interests of highway safety and to comply with policy T19 of the North Lincolnshire Local Plan, policies CS18 and CS19 of the North Lincolnshire Core Strategy, and paragraphs 159 to 169 of the National Planning Policy Framework.

10.

If, during development, any odorous, discoloured or otherwise visually contaminated material is found to be present at the site then no further development shall be carried out

until a written method statement detailing how this contamination shall be dealt with has been submitted to and approved in writing by the local planning authority.

Reason

To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other off-site receptors in accordance with policy DS7 of the North Lincolnshire Local Plan.

11.

Construction, demolition, and site clearance operations shall be limited to the following days and hours:

- 8am to 6pm Monday to Friday

- 8am to 1pm on Saturdays.

No construction, demolition or site clearance operations shall take place on Sundays or public/bank holidays.

HGV movements shall not be permitted outside these hours during the construction phase without prior written approval from the local planning authority.

Installation of equipment on site shall not be permitted outside these hours without prior written approval from the local planning authority.

Reason

To define the terms of the permission and in the interests of safeguarding residential amenity.

12.

The development hereby permitted shall be carried out in accordance with the submitted Flood Risk Assessment and Addendum Section 12 (May 2022 by Howard J Wroot Chartered Surveyor) and the following mitigation measures it details:

- Finished floor levels will be set at 4.4 metres above Ordnance Datum (AOD), providing a freeboard of 300 millimetres above the critical flood level (CFL) of 4.1 metres AOD.

- First floor levels will be set at 7.0 metres AOD affording an area of safe refuge.

- Flood-proofing measures will be implemented in accordance with Section 6 of the submitted FRA to a height of at least 1200 millimetres above finished floor levels, giving flood proofing to a level of 5.6 metres AOD.

These mitigation measures shall be fully implemented prior to occupation and subsequently be retained and maintained thereafter throughout the lifetime of the development.

Reason

To reduce the risk of flooding to the proposed development and future occupants.

13.

Before any dwelling is first occupied, the bathroom windows in the first floor of the eastern and western elevations of the terrace of houses hereby permitted shall be obscure glazed to a minimum of Privacy Level 3 in accordance with the Pilkington Scale of Obscuration and shall be retained in that condition thereafter.

Reason

To protect the living conditions presently enjoyed by the occupants of adjoining properties in accordance with policy DS1 of the North Lincolnshire Local Plan.

14.

Before any dwelling is first occupied, the bedroom windows in its western and eastern elevations on the second floor (within the roof space) shall be obscure glazed to a minimum of Privacy Level 4 in accordance with the Pilkington Scale of Obscuration and shall be retained in that condition thereafter.

Reason

In order to protect the living conditions presently enjoyed by the occupants of adjoining properties in accordance with policy DS1 of the North Lincolnshire Local Plan.

15.

No above-ground works shall take place until a method statement for the no-dig driveway construction to be undertaken in the root protection zone shown on Plan 2 (Tree Protection Plan) in the Tree Survey and Tree Protection Plan March 2022 submitted with the planning application has been submitted to and approved in writing by the local planning authority. The statement shall include the method of no-dig construction measures to be used, details of the depth of construction and the long-term maintenance of the driveway. The works shall take place in accordance with the agreed details and shall thereafter be retained.

Reason

To ensure the work is carried out in a correct manner to protect the health of the trees and their contribution to the visual amenity and character of the area.

16.

The development hereby permitted shall be carried out in strict accordance with the method and extent of tree protection measures detailed in the Tree Survey and Tree Protection Plan dated March 2022, including the method and extent of tree protection identified on Plan 2 (Tree Protection Plan). The tree protection measures shall be retained for the duration of the works. Within the areas so fenced off, the existing ground level shall be neither raised nor lowered (except as may be approved by the local planning authority as part of the development) and no materials, equipment, machinery or temporary buildings, or surplus soil, shall be placed or stored thereon. If any trenches for services are required in the fenced-off areas, they shall be excavated and backfilled by hand and any tree roots encountered with a diameter of 5 centimetres or more shall be left unsevered.

Reason

To ensure the work is carried out in a correct manner to protect the health of the trees and their contribution to the visual amenity and character of the area.

17.

Within three months of the commencement of development, the applicant or their successor in title shall submit a biodiversity management plan to the local planning authority for approval in writing. The plan shall include:

- (a) details of bat roosting features to be installed;
- (b) details of nesting sites to be installed to support house sparrows;
- (c) restrictions on lighting to avoid impacts on bat roosts, bat foraging areas, bird nesting sites and sensitive habitats;
- (d) provision for hedgehogs to pass through any fencing installed between gardens and between areas of grassland;
- (e) prescriptions for the planting and aftercare of trees of high biodiversity value in accordance with the submitted Small Sites Metric revision 1;
- (f) proposed timings for the above works in relation to the completion of the buildings.

Reason

To conserve and enhance biodiversity in accordance with policies CS5 and CS17 of the Core Strategy.

18.

The biodiversity management plan shall be carried out in accordance with the approved details and timings, and the approved features shall be retained thereafter, unless otherwise approved in writing by the local planning authority. The applicant or their successor in title shall submit photographs of the installed bat roosting and bird nesting features to the local planning authority, within two weeks of installation, as evidence of compliance with this condition.

Reason

To conserve and enhance biodiversity in accordance with policies CS5 and CS17 of the Core Strategy.

Informative 1

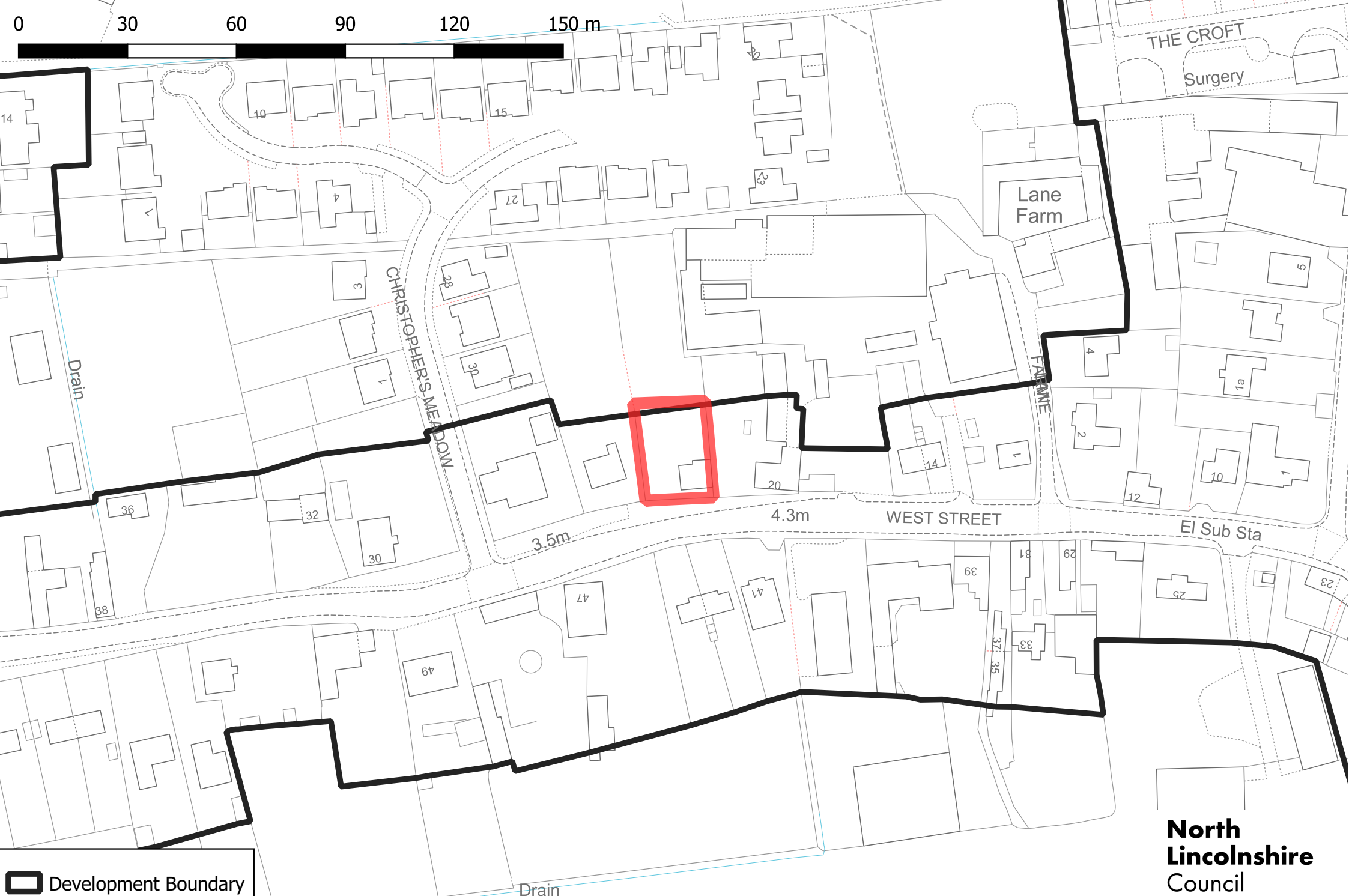
The development hereby granted planning permission requires works to be carried out within the limits of the adopted (public) highway. Therefore:

- before ANY construction works take place within the limits of the highway you MUST contact the highway authority on telephone number 01724 297000 to arrange for the relevant permissions/licenses to be issued;
- before ANY service (utility) connections take place within the limits of the highway you MUST contact the highway authority on telephone number 01724 297319 to arrange for the relevant permissions/licenses to be issued.

Informative 2

In determining this application, the council, as local planning authority, has taken account of the guidance in paragraph 38 of the National Planning Policy Framework in order to seek to secure sustainable development that improves the economic, social and environmental conditions of the area.

0 30 60 90 120 150 m



 Development Boundary

PA/2021/2240

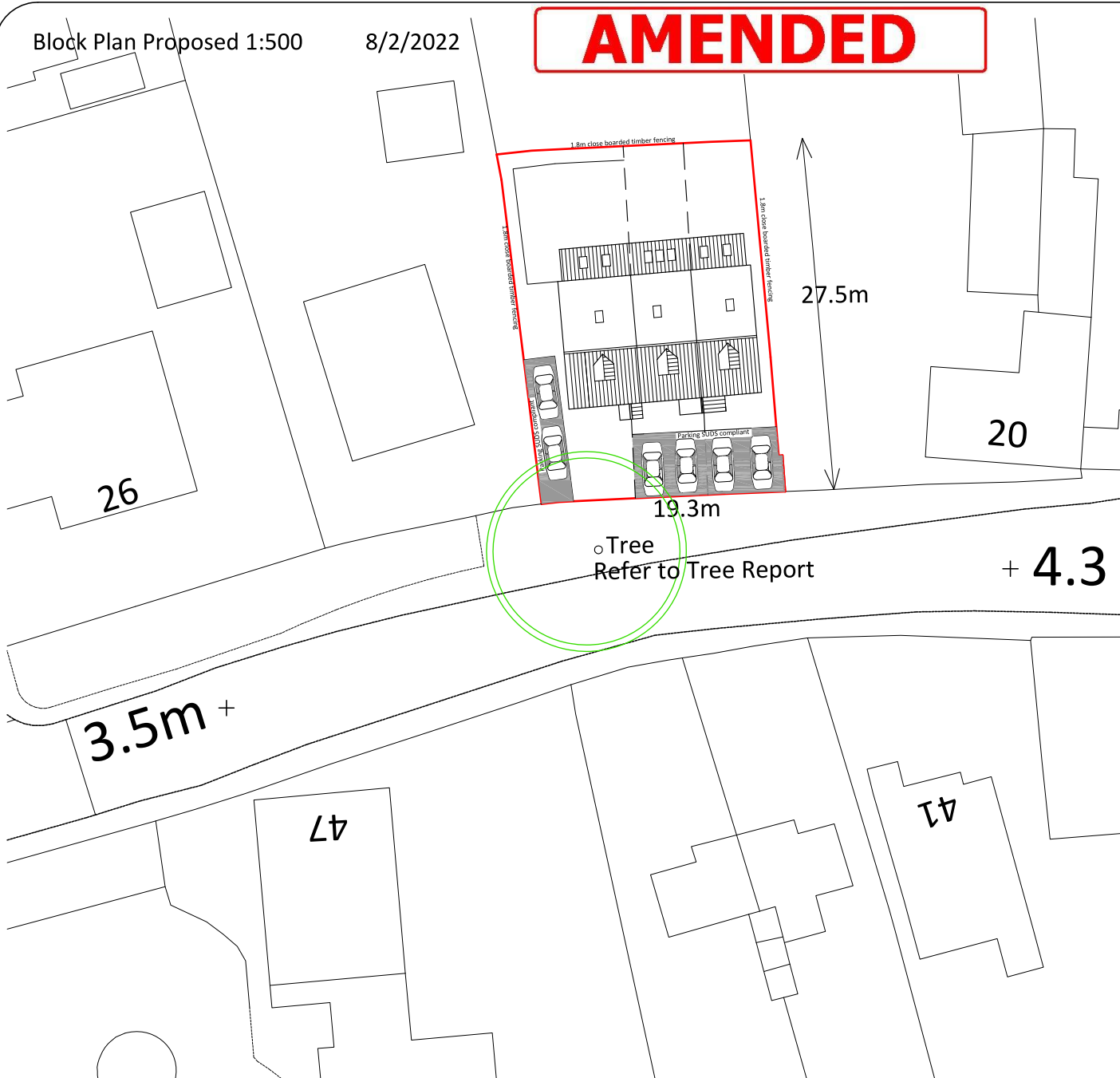
**North
Lincolnshire
Council**

PA/2021/2240 Proposed layout (not to scale)

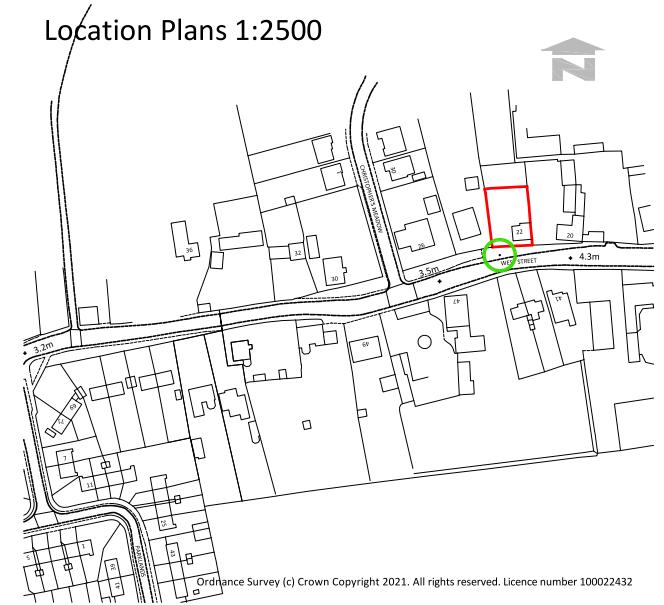
AMENDED

Block Plan Proposed 1:500

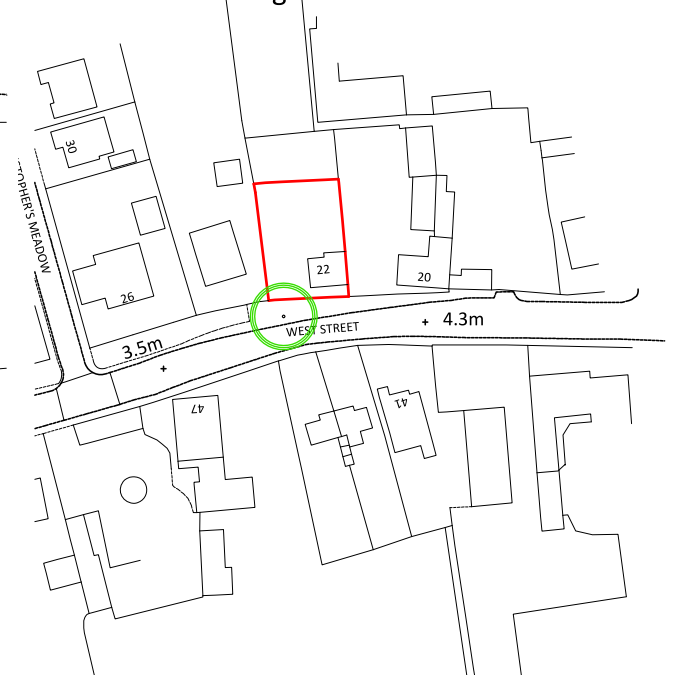
8/2/2022



Location Plans 1:2500



Block Plan Existing 1:1250



Howard J Wroot BSc MRICS
Chartered Surveyor
240 Wharf Road, Ealand
Scunthorpe DN17 4JN

Tel 01724 711068
Fax 01724 710174
Mob 07947 226577

Client	Cleveland Build		
Date	12/12/21	Dwg No	4 of 4
Scale	As noted	Ref No	1078-21

Proposal
Drawing

Development at 22 West Street, West
Butterwick, Scunthorpe, DN17 3LA
Location and Block Plans

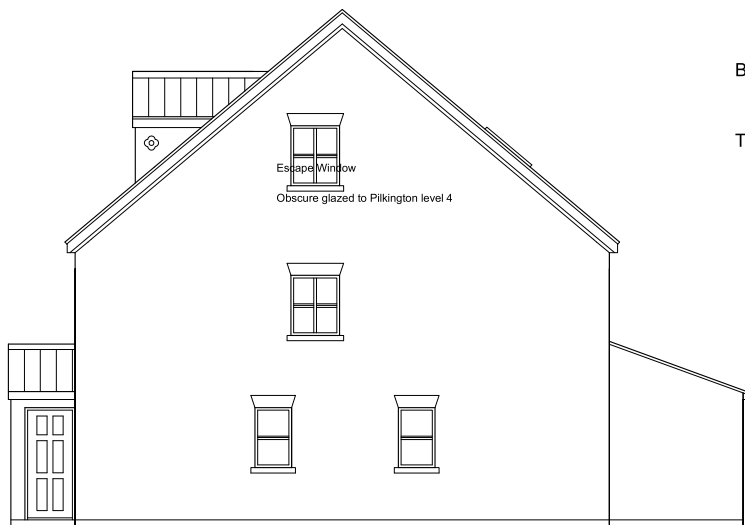


PA/2021/2240 Proposed elevations (not to scale)

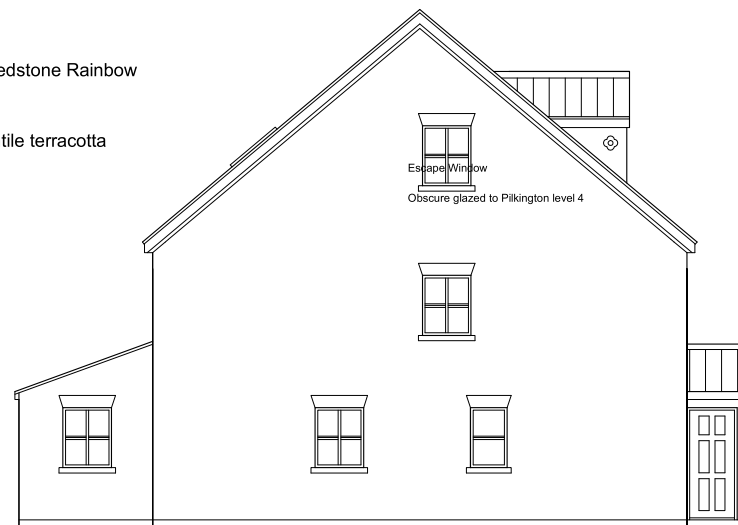
9/9/2022

Bricks - Weinerberger Redstone Rainbow

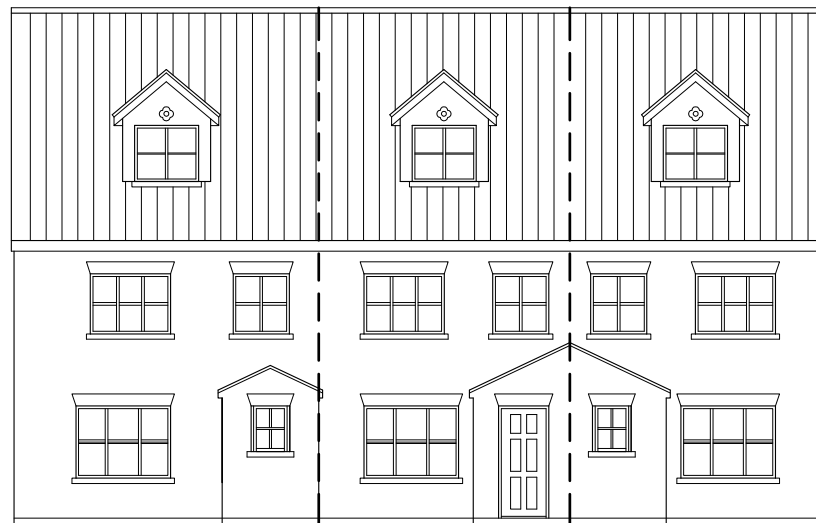
Tiles - Sandtoft Neo-Pantile terracotta



Right Side Elevation



Left Side Elevation



Front Elevation



Rear Elevation

Howard J Wroot BSc MRICS
Chartered Surveyor
240 Wharf Road, Ealand
Scunthorpe DN17 4JN

Tel 01724 711068
Fax 01724 710174
Mob 07947 226577

Client	Cleveland Build		
Date	12/12/21	Dwg No	3 of 4
Scale	1:100	Ref No	1078-21

Proposal	Terrace of 3 houses at 22 West Street West Butterwick, Scunthorpe, DN17 3LA
Drawing	Elevations

