

APPLICATION NO	PA/2023/1236
APPLICANT	Bellway Homes (Yorkshire Division), Francis Day, Richard Day and Jalapeno Ventures Ltd
DEVELOPMENT	Planning permission for a residential development (Use Class C3) with associated works, including highways, open space, landscaping and drainage infrastructure
LOCATION	Land off Wrawby Road, Brigg
PARISH	WRAWBY
WARD	Brigg and Wolds
CASE OFFICER	Tanya Coggon
SUMMARY RECOMMENDATION	Subject to S106, approve with conditions
REASONS FOR REFERENCE TO COMMITTEE	Objection by Wrawby Parish Council Significant public interest Departure from the development plan

POLICIES

National Planning Policy Framework:

- 2 Achieving sustainable development
- 4 Decision-making
- 5 Delivering a sufficient supply of homes
- 8 Promoting healthy and safe communities
- 9 Promoting sustainable transport
- 11 Making effective use of land
- 12 Achieving well-designed and beautiful places
- 14 Meeting the challenge of climate change, flooding and coastal change
- 15 Conserving and enhancing the natural environment
- 16 Conserving and enhancing the historic environment

North Lincolnshire Local Plan:

- H5 New housing development

H8 Housing design and housing mix
H10 Public open space provision in new housing development
RD2 Development in the open countryside
T1 Location of development
T2 Access to development
T6 Pedestrian routes and footpaths
T8 Cyclists and development
T19 Car parking provision and standards
C1 Educational facilities
LC5 Species protection
LC6 Habitat creation
LC15 Landscape enhancement
LC12 Protection of trees, woodland and hedgerows
HE9 Archaeological excavation
DS1 General requirements
DS3 Planning out crime
DS7 Contaminated land
DS14 Foul sewage and surface water drainage
DS15 Water resources
DS16 Flood risk

North Lincolnshire Core Strategy:

CS1 Spatial strategy for North Lincolnshire
CS2 Delivering more sustainable development
CS3 Development limits
CS5 Delivering quality design in North Lincolnshire
CS6 Historic environment
CS7 Overall housing provision
CS8 Spatial distribution of housing sites

CS9 Affordable housing

CS16 North Lincolnshire's landscape, greenspace and waterscape

CS17 Biodiversity

CS18 Sustainable resource use and climate change

CS19 Flood risk

CS22 Community facilities and services

CS23 Sport, recreation and open space

CS24 Health care provision

CS25 Promoting sustainable transport

CS27 Planning obligations

Housing and Employment Land Allocations DPD 2016:

PS1 Presumption in favour of sustainable development

BRIH-3: Land at Wrawby Road Phase 2 (11.53ha) is allocated for housing (333 dwellings).

BRIH-4: Land at Wrawby Road (4.29 ha) is allocated for housing (152 dwellings).

New North Lincolnshire Local Plan Submission: The new North Lincolnshire Local Plan was submitted for public examination to the Planning Inspectorate on 11 November 2022. Examination of the Plan has therefore commenced, although public hearing sessions are not anticipated until late 2024.

The Submitted North Lincolnshire Local Plan can be given some weight as a material planning consideration in the determination of planning applications. The relevant policies concerning this application are:

SS1 Presumption in favour of sustainable development

SS2 A spatial strategy for North Lincolnshire

SS3 Development principles

SS5 Overall housing provision

SS6 Spatial distribution of housing sites

SS11 Development limits

H1 Site allocations

H1P-16 Land at Wrawby Road Phase 2 (11.97ha) is allocated for housing (333 dwellings)

H1P-17 Land at Wrawby Road Phase 1 (4.31ha) is allocated for housing (152 dwellings)

H2 Housing mix and density

H3 Affordable housing

RD1 Supporting sustainable development in the open countryside

HE1 Conserving and enhancing the historic environment

TC2 Placemaking and good urban design

DQE1 Protection of landscape, townscape and views

DQE3 Biodiversity and geodiversity

DQE5 Managing flood risk

DQE6 Sustainable drainage system

DQE7 Climate change and low carbon living

DQE11 Green infrastructure network

DQE12 Protection of trees, woodland and hedgerows

CSC1 Health and wellbeing

CSC3 Protection and provision of open space, sports and recreation facilities

CSC10 Community facilities and services

T1 Promoting sustainable transport

T2 Promoting public transport

T3 New development and transport

T4 Parking

T5 Cycle and motorcycle parking

DM1 General requirements North Lincolnshire

DM3 Environmental protection

ID1 Delivering infrastructure

Other relevant legislation and guidance:

Levelling up and Regeneration Act (LURA) 2023

SUDs and Flood Risk Supplementary Planning Guidance (April 2017)

Planning for Health and Wellbeing Supplementary Planning Guidance (November 2016)

Interim Planning Policy – Affordable Housing (August 2015)

SPG8 Developer Contributions to Schools

SPG10 Provision of Open Space in New Housing Developments

Housing and Economic Needs Assessment 2020

North Lincolnshire Residential Road Design Guide

DfT Circular 01/2022 Strategic road network and the delivery of sustainable development

CONSULTATIONS

Highways: The site is allocated in the adopted Housing and Employment Land Allocations DPD and the proposed development is acceptable in principle from a highways perspective. The site will be accessed via a new priority T-junction with Wrawby Road and will deliver a section of the Brigg link road as the main access route into the site. My previous response requested further information/clarification on identified issues. Active Travel England also raised various issues. The applicant issued a Technical Note addressing these issues and I have also had further meetings with Active Travel England and the applicants to discuss the responses.

The key points raised in our previous response were:

Additional footway/cycleway connection extending beyond plots 244 and 249 to connect into Wrawby Road in the vicinity of Churchill Avenue, to improve connectivity to/from the site for pedestrians and cyclists: The planning layout (Drawing number 2250.01 Rev R) has been amended to show the provision of this pedestrian/cycle link.

Measures to prevent vehicular access to/from South View Avenue (other than in an emergency): The applicant has confirmed that measures will be implemented to prevent vehicular access to/from South View, other than in an emergency. The details of these measures will be secured via a suitably worded condition.

Improvements to the existing footway/cycleway across the site frontage: The applicant has confirmed they are happy to accept a suitably worded condition regarding the improvements to the existing footway/cycleway along the site frontage. They will also provide a priority pedestrian/cycle crossing on the site access arm in line with LTN 1/20. This will be sufficiently set back to ensure that vehicles don't overhang onto the A18. The details of this will be conditioned and agreed as part of the detailed design for the site access. Having reviewed the additional information provided, I am satisfied that the applicant has adequately addressed the comments raised previously.

The key points raised by ATE in their latest response, dated 27th February 2024 were:

Access and footway/cycleway: The developer is intending to install a priority crossing at the site access, which addresses ATE's concerns regarding the access. The developer has agreed to improve the footway/cycleway along the site frontage, which will be secured via a suitably worded condition. ATE's comments regarding the recommended design standards are noted.

Link road crossing facilities: It is recognised that once the link road is fully completed from Wrawby Road to Atherton Way and daily traffic flows increase, that there is the potential for severance issues. This has been discussed with the developer and they have agreed to

provide a parallel crossing on the link road, which will be secured via a suitably worded condition.

Connections to residential areas to the south: It is acknowledged that Wrawby Road is a busy road with no controlled crossing facilities. The provision of a controlled facility would significantly improve connectivity for people crossing Wrawby Road. However, such a facility would offer more benefit to existing residents and it is difficult to make a direct argument for it being required as a direct result of the development. It would therefore be difficult to justify on planning grounds.

Cycle parking: Having discussed this with the developer, they are amenable to providing a secure cycle parking (such as a small shed) for all dwellings without a garage. This will be secured via a suitably worded condition.

I am therefore satisfied that the additional information provided by the applicant has addressed the comments raised by the council and ATE. I note that National Highways have recommended conditions to be attached to any planning permission that may be granted. As this includes a condition for a construction traffic management plan, I have not suggested a similar condition. Recommend conditions in relation to a phasing plan, vehicle access and parking, improvements to the footway/cycleway across the site frontage, no loose material shall be placed on any driveway or parking area, details of private driveways, provision of wheel cleaning facilities, details of the pedestrian/cycle linkages onto South View Avenue and Wrawby Road, cycle parking for each dwelling, provision of a controlled pedestrian/cycle crossing of the Brigg link road, details of access roads, visibility, no development within any service strip adjacent to any shared surface road and the submission of a travel plan.

National Highways: Recommend conditions be attached to any planning permission that may be granted. These conditions relate to this application and the pending application PA/2023/1425, and require the submission of a construction phased traffic management plan and a boundary treatment plan for the M180 before commencement of development.

Active Travel England (ATE): [Summarised] Recommends deferral: ATE is not currently in a position to support this application and requests further assessment, evidence, revisions and/or dialogue as set out in this response. ATE considers that the application as submitted does not demonstrate that 'appropriate opportunities to promote sustainable transport modes can be – or have been – taken up' or gives priority first to pedestrian and cycle movements in accordance with National Planning Policy Framework (NPPF) paragraphs 114 and 116. It is therefore recommended that this application should not be determined until further information has been submitted and reviewed. If the council is minded to grant consent, conditions for details of the site access points on to South View Avenue and Wrawby Road for pedestrians and cyclists are submitted to the council, details of residential cycle parking to be submitted to and approved by the council, submission of a general arrangement plan(s) showing a suitable, controlled, pedestrian or pedestrian/cycle crossing of Wrawby Road and submission of a general arrangement plan(s) showing a suitable, controlled, pedestrian or pedestrian/cycle crossing of the proposed spine road (also known as the Brigg link road).

ATE welcomes the changes to the site layout to provide more pedestrian and cycling connections within the site, and the pedestrian and cycling accesses to South View Avenue and Wrawby Road shown in principle on drawing number 2250/2250/01 Rev R. It is recommended that the accesses are conditioned with an appropriate trigger to ensure they

are provided at the earliest opportunity to benefit residents from the outset. ATE welcomes the commitment to providing cycle parking at the play area. The commitment to widening the footway/cycleway along the site frontage and changes to the access are also welcome, but please see the comments below concerning widths.

ATE has used the TRICS database to generate multi-modal data for the site. From this it can be seen that the site is estimated to generate 62 public transport trips (which will start with walking), 166 pedestrian trips and 26 cycling trips. This is without any travel plan targets being applied. It is also important to put it into the context of the whole allocation: 820 dwellings is estimated to generate 179 public transport trips (which will start with walking), 471 pedestrian trips and 75 cycling trips. Again, this is without applying travel plan targets. No additional information concerning the quality of routes has been provided, although as stated below, we welcome the proposed widening of the footway/cycleway along the southern frontage of the site. In order to provide a real choice for residents, high-quality and safe infrastructure is required. Given that this site is part of a wider allocation, ATE recommends that a strategic approach is taken and schemes be identified that can be delivered through contributions or works that are proportionate to each land parcel.

ATE welcomes the changes to the site access to provide a priority crossing for pedestrians and cyclists and the provision of the widened footway/cycleway along the site frontage with Wrawby Road/A18. However, it is not clear that the proposed footway/cycleway conforms to LTN 1/20 standards. Given the context ATE does not object to a shared use footway/cycleway; however, clarity is required concerning the width of verge to be provided, and proximity of any hedge or fence on the northern side. LTN 1/20 Table 5-3 provides information on the additional width required due to fixed objects. Much of the route is currently in a 40mph speed limit therefore the recommended horizontal separation from the carriageway is 1m, while in the 30mph speed limit area a 0.5m separation is recommended (Table 6-1). The extension of the current 30mph speed limit to incorporate the residential frontage and vehicular access is recommended and should also be discussed with the highway authority.

ATE maintains that the link road will cause severance within the site, having a 30mph speed limit, no traffic calming and average daily flows estimated to be over 4,000 vehicles per day. Therefore, ATE considers that a controlled crossing such as a parallel or zebra crossing should be included in the design of the road, in accordance with Table 10-2 of LTN 1/20, to provide a provision suitable for most people. It should be situated to serve the north and south of the site and make the shortest connections possible to the access on South View Road and to the play areas.

ATE recognises that there are uncontrolled crossings on Wrawby Road/A18; however, it is understood that the average annual daily flow on is approximately 8,000 vehicles and the speed limit is 30mph to west of the site. The pedestrian and vehicular movements will also increase with the proposed development. Therefore, a controlled crossing such as a parallel or zebra crossing should be provided in accordance with Table 10-2 of LTN 1/20 to provide a provision suitable for most people. This will provide an important connection between the residential areas and a quieter route for cyclists to the town centre. It will also allow people to cross the road between the existing bus stops, making their use safer. ATE recommends that the developer, in consultation with the highway authority, submits a drawing showing a controlled crossing in principle that can then be conditioned and delivered through a Section 278 agreement at the same time as the access.

ATE welcomes the commitment to providing cycle parking at the play area. ATE accepts that that cycle parking can be accommodated in the garage for those dwellings that have one. However, where there is no garage, it is not accepted that cycles should be stored in the house. If cycles are stored in areas of the house such as the hallway they can block the route and be a hazard in the event of a fire, while in other areas they can be a nuisance. Therefore, this approach is likely to actively discourage cycling and mean that targets in the travel plan will not be met. ATE is aware of many Bellway developments across the country where storage such as a shed or secure and covered storage is provided, and recommends that that same is provided at this development to conform with LTN 1/20.

PROW: No public rights of way are directly affected by the proposal. Public Footpath 293 is a popular walk, which follows the lane leading north-west from the A18 in the direction of the M180, passing to within approximately 80m only of the site's north-eastern corner. Although we note the land between FP293 and the site is outside the applicant's holding (or so we assume), even so we should like to see the creation of a new footpath, if at all possible, linking the site and FP293. We believe doing so would be in accordance with paragraph 100 of the National Planning Policy Framework and Core Strategy CS25 of the North Lincolnshire Local Development Framework. This, we believe, could be accommodated within the site within the proposed landscaped area south-east of the proposed lake.

Environment Agency: No objections. Advises that the site is within the Brigg sewerage catchment; based on the submitted flood risk assessment and drainage strategy plan, we would expect the development to be connected to the mains sewerage network. However, we note that Anglian Water's response, dated 04 August 2023, suggests there is no connection to the Anglian Water sewers. We therefore strongly recommend you re-consult Anglian Water for further comments and confirm the intended drainage strategy with the applicant if necessary. Whilst the data available to us indicates there is headroom to accommodate this development, it shows that the Brigg Water Recycling Centre is under stress due to the measured Q90 flow being above 80% of the permitted dry weather flow for the past five years. The applicant will therefore need to continue to work with Anglian Water to ensure appropriate measures are in place to take the foul flows prior to occupation of the dwellings.

LLFA Drainage: Recommends pre-commencement planning conditions requiring a surface water drainage strategy, and details of how surface water will be prevented from discharging onto the highway from the site and vice versa, to be submitted to the council for approval.

Anglian Water:

Original response: No connection to the Anglian Water sewers; we therefore have no comments.

Updated response:

Used Water Network: The foul drainage from this development is in the catchment of Brigg Water Recycling Centre that will have available capacity for these flows. Whilst the proposed connection as detailed in the submitted documents is acceptable in principle, there are capacity constraints within the network. Consequently the full development may lead to an unacceptable risk of flooding and/or pollution. Anglian Water will need to plan effectively for the proposed development, if permission is granted. We will need to work

with the applicant to ensure any infrastructure improvements are delivered in line with the development. Anglian Water are currently monitor[ing] the network. Further analysis will be required to establish the extent of network reinforcement that may be required to accommodate the full development. We will need to engage with the applicant throughout this process to understand timescales. We therefore request a condition requiring a phasing plan and/or an on-site drainage strategy.

Surface Water Disposal: The preferred method of surface water disposal would be to a sustainable drainage system (SuDS) with connection to sewer seen as the last option. Building Regulations (part H) on Drainage and Waste Disposal for England includes a surface water drainage hierarchy, with infiltration on site as the preferred disposal option, followed by discharge to watercourse and then connection to a sewer. From the details submitted to support the planning application, the proposed method of surface water management does not relate to Anglian Water operated assets. As such, we are unable to provide comments on the suitability of the surface water management.

Shire Group IDB: We are pleased with the amendments that highlight the 9m easement with respect to the ordinary watercourse. If the surface water were to be disposed of via a soakaway system, the IDB would have no objection in principle but would advise that the ground conditions in this area may not be suitable for soakaway drainage. It is therefore essential that percolation tests are undertaken to establish if the ground conditions are suitable for soakaway drainage throughout the year. If surface water is to be directed to a mains sewer system, the IDB would again have no objection in principle, providing that the water authority are satisfied that the existing system will accept this additional flow. If the surface water is to be discharged to any ordinary watercourse within the drainage district, consent from the IDB would be required in addition to planning permission and would be restricted to 1.4 litres per second per hectare or greenfield run-off. No obstructions within 9m of the edge of an ordinary watercourse are permitted without consent from the IDB. If surface water or works are planned adjacent to a main river within the drainage district, then the Environment Agency should be contacted for any relevant permits. Recommends conditions that if consent is required from the IDB this is conditioned, and should on-site SuDS or flow restriction be proposed as part of any larger development, the IDB requests that those restricted flow measures or attenuation are put in place before occupancy and within three months of development progressing on site.

Strategic Housing:

Original response: Policy CS7 of the Core Strategy states that all proposals for housing should include a variety of housing types, sizes and tenures to meet the local housing needs. Our most recent housing need assessment 2020 concludes that in North Lincolnshire our need is as follows:

- 1 bedroom = 8.1%
- 2 bedroom = 22.5%
- 3 bedroom = 53.1%
- 4+ bedroom = 16.3%.

Currently your proposed application is the following mix:

- 2 bedroom = 1.4%
- 3 bedroom = 53.8%
- 4 bedroom = 44.8%.

Our local housing need assessment demonstrates 30.6% of new homes should be 1- or 2-bed and for this proposal it is only 1.4% which equates to 4, 2-bed units, all of which are bungalows. This is an unacceptable mix. We are requesting a further 35, 2-bed units which will result in the following mix:

- 2 bedroom = 13.4%
- 3 bedroom = 41.7%
- 4 bedroom = 44.8.

Response to amended plans: New proposed bedroom mix: 2-bed properties = 39 units, 3-bed properties = 121 units, 4-bed properties = 130 units. I have reviewed the above proposed bedroom mix and taking consideration of the market conditions, economic viability and the site-specific considerations submitted as part of the Supplementary Note for Density and Housing Mix, we are agreeable to the above housing mix.

S106 Officer: Due to the submitted viability report to provide zero S106 contributions, the below contributions are subject to negotiation following an independent viability review and may differ depending on the results of this independent review. Contributions would be required for affordable housing, education, recreation, informal open space, area of play, biodiversity net gain (if this needs to be off-site), highway/cycleway/footway along Wrawby Road, and health and public transport.

Education: We seek both primary and secondary contributions in respect of this development. The amount per eligible dwelling for both primary and secondary places is £10,602. (This figure is valid for S106 agreements signed by 31/03/2024.)

Environmental Protection:

Noise: An environmental noise survey at the site was undertaken between 8 and 10 June 2022. Measurements were collected at two unattended measurement locations; the dominant noise source at the site was established as being road traffic noise from the M180 and Wrawby Road.

The initial ProPG Stage 1 Assessment finds that acoustic challenges at the site are of low to medium risk. The ProPG Stage 2 Assessment reports that the site is located a large distance from the M180 with future residential premises expected to be present in the future within the proposed development site to the north (subject to a separate planning application). This would result in reduced noise levels in the northern part of the site due to screening from the M180. The report includes an Acoustic Design Statement and the design of the site includes a stand-off to Wrawby Road with the first row of buildings facing Wrawby Road orientated so that the buildings provide a barrier to rear gardens and dwellings located further into the site.

In order to meet internal noise levels, an example glazing and ventilator strategy has been identified to meet the relevant BS8233/WHO criteria in proposed internal habitable spaces.

It is reported that a scheme of sound insulation would be confirmed at later design stages which would also include consideration of meeting the requirements of the Building Regulations Approved Document O: Overheating. It is suggested that the requirement to provide a suitable scheme of sound insulation for approval by the local authority can be secured by a suitably worded planning condition. Recommends planning conditions relating to the submission of a noise mitigation scheme for approval to the council and the submission of a verification report to the council.

Contamination: The site currently comprises five agricultural fields which are separated by drainage ditches, hedges and trees. The site is approximately 24ha in size. The historical maps dating back to 1886 identify that the site has been used as agricultural land and has not been subjected to any former industrial land use; however, arable farming activities may have given rise to some potential contamination on site. The site lies in a low risk radon area and therefore no radon protection measures are required. The preliminary conceptual site model has identified several potential contaminants associated with different farming activities over the years which may give rise to contamination of the soils. As the proposed end use (residential) is of a sensitive nature, the report recommends an intrusive investigation is required. Recommends a full contamination condition.

Air quality: The submitted air quality assessment considers the air quality impacts from the following sources: fugitive dust emissions during construction and road traffic exhaust emissions associated with vehicles travelling to and from the site during operation. The conclusion of this assessment is that the potential risk of dust soiling is high from earthworks, construction and trackout activities. The potential risk of human health impacts is medium from earthworks, construction and trackout activities. Recommends conditions relating to restrictions on construction, demolition and site clearance operations and the submission of a CEMP to the council for approval.

Archaeology: The applicant has undertaken archaeological field evaluation in accordance with our advice and has submitted a report of the results with the application. The field evaluation comprised geophysical survey followed by the excavation of trial trenches to confirm the survey results. Previous evaluation results held in the HER are also taken into account. No significant archaeological features have been identified across the application site and the residual potential for the presence of as yet unrecorded archaeological remains is judged to be low. No further archaeological work in advance of or during the proposed development is necessary. No further recommendations for archaeology.

Conservation: Wrawby Road has a variety of building styles and characteristics. Closer to the town centre this character is late Victorian/Edwardian; heading further out of town, the age of building becomes more modern, much of which is mid-20th century good quality inter-war housing sharing some characteristics of the Edwardian buildings nearer the centre. The last of these buildings is 33 Wrawby Road which sits directly adjacent the proposal site and on the south side of Wrawby Road number 54. After this point the housing is limited to the south side of Wrawby Road and is of later date, likely 1970/80s and is of no architectural merit.

If the application is supportable, consideration should be given to revising the design of dwellings, particularly those fronting Wrawby Road, to integrate more sympathetically with the better-quality buildings described above. This should be reflected in the form and details, with thought given to an overall more traditional design and materials through use of red brick/slates/pantiles/render/sash-style windows in the Victorian/Edwardian style (2over2 and 1over1). At present, the design of many of the new dwellings put forward does not

meet the requirement to 'respect and reflect the form, scale, massing, design and detailing, materials, and nature of the local environment' of policy H8 of the adopted local plan and will add yet a further design variant to the area that would be visually discordant in the context of surrounding houses with the later development of Churchill Avenue being an example of this.

Trees: There is a need on this application for submission of an arboricultural report as there are many trees, particularly a line of large trees on the site, that have visual amenity, and all of these, plus other trees, need to be properly considered as part of the proposal. The indicative landscaping does not seem to give clarity over the trees and their fate if consent were to be given. There are significant trees here which, due to their significance on the landscape, are worthy of a TPO and I would be concerned about the loss of them on this development, particularly G73 which have been considered category B trees and they are suggesting that they will all be removed. These trees are high quality trees and therefore should leave sufficient space around the trees so they will not unduly impact on the dwellings once built. I object to the application.

Ecology: [Summarised] The proposals will have a significant impact on the landscape at the local level, but appropriate landscape enhancements have been proposed. Standard sensitive working methods will be required for protected and priority species. Planning conditions are proposed to minimise harm to protected and priority species and habitats and to seek a measurable net gain in biodiversity in accordance with policy CS17, the National Planning Policy Framework and Biodiversity Metric 3.0.

Waste and Recycling: General advice on vehicle access for a refuse vehicle, highway construction, unadopted roads, bin types and dimensions, pulling distances for crews and residents, refuse and recycling storage, and composting.

Humberside Police: I am delighted to see that the applicant has fully considered Designing out Crime and Crime Prevention through Environmental Design (CPTED) features throughout the proposal. I can confirm that I have no objections.

Humberside Fire and Rescue Service: It is a requirement of Approved Document B5, Section 15 Commercial Properties or B5, Section 13 for Domestic Premises that adequate access for fire-fighting is provided to all buildings or extensions to buildings. Where it is a requirement to provide access for high-reach appliances, the route and hard standing should be constructed to provide a minimum carrying capacity of 24 tonnes. Adequate provision of water supplies for fire-fighting appropriate to the proposed risk should be considered. If the public supplies are inadequate it may be necessary to augment them by the provision of on-site facilities. Under normal circumstances hydrants for industrial unit and high-risk areas should be located at 90m intervals. Where a building which has a compartment of 280m² or more in the area is being erected more than 100m from an existing fire hydrant, hydrants should be provided within 90m of an entry point to the building and not more than 90m apart. Hydrants for low-risk and residential areas should be located at intervals of 240m.

PARISH/TOWN COUNCILS

Wrawby Parish Council: Objection. Concerns that the infrastructure will be unable to cope. Doctor and dentist appointments are difficult to secure already. The proposals are on a greenfield site that will see the removal of established trees and hedgerows. The noise and air pollution in this area will increase along with a detrimental impact on bats,

hedgehogs and other wildlife. Increased traffic onto and out of the A18 Wrawby Road especially when turning right from the development in the direction of Brigg. Concerns regarding the negative impact on both the surface water and foul drainage in the area. Should this application be approved there is a perfect opportunity for the concerns regarding the narrowing of the footpath and cycleway on the A18 as the point it enters Brigg to be addressed. The footpath and the cycleway needs to be widened to allow both pedestrians and cyclists to travel safely between Brigg and Wrawby.

Brigg Town Council: Concerns that the infrastructure will be unable to cope. Doctor and dentist appointments are very difficult to secure already. The proposals are on a greenfield site that will see the removal of established trees and hedging. The noise and air pollution in this area will increase along with a detrimental impact on bats, hedgehogs and other wildlife. Increased traffic onto and out of A18 Wrawby Road. Concerns regarding the negative impact on both the surface and foul drainage in the area.

PUBLICITY

The application, including all the amended plans, has been advertised by site and press notices. Twenty-five letters of objection and two letters of support in response to the original and amended plans have been received raising the following matters:

- infrastructure unable to cope
- increase in traffic
- loss of a greenfield site
- lack of public transport
- increased noise and vibration
- increased pressure on existing services
- no affordable housing
- no funding for amenities by an s106 agreement
- loss of poplar trees
- loss of wildlife
- over-development
- out of character
- lack of notification/public engagement
- inadequate numbers of bungalows proposed
- the site provides drainage relief in times of flooding
- loss of privacy

- Grammar School Road should not be used as an entry/exit for the development
- loss of polar trees may cause stray cricket balls and footballs into the residential properties of the development.

The letters of support raises the following issues:

- support if infrastructure is in place
- housing limits will be met and stop building outside the building line
- will allow the town to prosper
- will provide housing.

STATEMENT OF COMMUNITY INVOLVEMENT

A statement of community involvement has been submitted with this application. A public exhibition took place at Brigg Heritage Centre on 13/10/2022. Brigg and Wolds ward councillors were contacted by email prior to the exhibition. 1,100 leaflets were distributed on 04/10/2022 by post to residents and businesses who lived and worked within proximity of the development site, notifying them of the forthcoming proposals, the public exhibition and how to make representations. A consultation flyer was sent to Brigg Town Council, Wrawby Parish Council and Brigg Heritage Centre. Brigg Heritage Centre also provided the consultation flyer to Brigg Tourist Information office, Brigg Library, Brigg Community Facebook Group, Brigg for Business Events and Community Facebook Group and Brigg Live Arts Facebook Group. A public notice containing details of the applicant's event was also posted in the Scunthorpe Telegraph circulating on 06/10/2022.

The public exhibition event was held from 11am to 7.30pm. Around 150 people attended the event and 79 respondents provided comments. A consultation website was made available from 14/10/2022 until the application was submitted. Seven people responded via the website.

In total 93 responses were received. The following questions were asked of consultees via the feedback form:

1. Do you live in Brigg?
2. What age are you?
3. Do you support new homes in Brigg?
4. What type of housing do you think is needed in Brigg?
5. Do you support the landscaping proposals which will be delivered as part of the scheme?
6. What aspects of the proposals do you like, and do you feel they could be improved in any way?

89% of respondents were from Brigg. Most of those who provided comments were aged 60+. 60/5% of respondents supported new housing in Brigg. The preference for the type of

housing was three-bedroom with a garden. Those who selected 'other' housing types wanted to see five-bedroomed, bungalows, starter homes, affordable homes and low energy/sustainable homes. 60% were supportive of the indicative landscaping scheme. For question 6 comments were made about doctor/dentist capacity, school capacity, highway concerns about Grammar School Road, highway concerns/comments about Wrawby Road, highway capacity concerns across Brigg, emergency services capacity, environmental matters, need for more amenities, flooding, utilities capacity, housing mix/type and open space provision.

The developer has provided a detailed response to the feedback from the consultation responses in the statement of community involvement. In terms of doctor/dentist and school capacity the developer will engage with the council to determine existing capacity and viability testing to establish if any financial contributions towards health and education will be required. Highway matters – transport modelling and transport assessments have been submitted with the application and number of trips. For Wrawby Road, a new access is proposed with a priority junction and ghost island to provide access for 400 dwellings. Emergency access is proposed off Southview Avenue. A Landscape Masterplan has been submitted with 10% BNG. The development will support existing amenities in Brigg. A flood risk assessment (FRA) and drainage strategy has been submitted to show the development will not increase flood risk elsewhere in Brigg and will not cause adverse impacts on drainage and foul infrastructure in the area. Viability testing is undertaken to determine the level of affordable housing to be provided, the Design and Assessment Statement sets out the design of the new homes, homes are designed to meet 2025 future homes and the development is mainly three-bedroom properties. The development provides a LEAP and open space accessible to all.

ASSESSMENT

Proposal

The proposal is for 290 dwellings. The housing mix has been amended through the life of the application and is now as follows: 39 two-bedroom, 121 three-bedroom and 130 four-bedroom. The houses are a mix of semi and detached properties with four bungalows. Excluding the bungalows, all dwellings are two-storey. There are 15 different house types within the housing mix with a full palette of materials. Each property has its own off-street car parking comprising two car parking spaces in the form of a driveway, integral garage or detached garage. Cycle storage is proposed within the garages.

The main site access is a new vehicular access from Wrawby Road with a ghost island on Wrawby Road. The proposals also involve the creation of a 'spine road' which includes segregated cycling and pedestrian footpaths that will link to the northern parcel of land for which there is a pending outline application for residential development (PA/2023/1425). This 'spine road' will form the new Brigg Link Road which will eventually link Wrawby Road to Atherton Way (once further phases of development across all the BRIH1-5 allocations are delivered). This application will therefore provide significant strategic infrastructure for Brigg and North Lincolnshire as the first part of Brigg Link Road will be delivered as part of this application.

A separate private drive access onto Wrawby Road will serve four dwellings to the south-west corner of the application site (plots 244 to 247). A pedestrian link into the site will be provided to the A18 Wrawby Road opposite the junction with Churchill Avenue and a further pedestrian link, cycle and emergency access will be provided to South View Avenue.

Various estate roads within the site will link to the main 'spine road' which forms the Brigg Link Road.

The scheme will provide approximately 0.6 hectares of accessible public open space. A small number of trees will be felled to facilitate the development but the poplar trees (G73) along the western boundary of the site with the recreation ground will be retained. Landscaping is proposed on the site and a landscaping master plan has been submitted with the application. The landscape master plan includes enhancing existing hedgerows, new hedgerows to plots and shrub planting, drainage swales and attenuation pond, native scrub planting, landscape buffers, neutral grassland and wet grassland.

The site

The site is approximately 11 hectares in area. The site where the dwellings are to be located is within the development limits of the market town of Brigg with some of the open space to the east side of the site lying outside the development boundary of Brigg. The site is approximately 1km from the town centre of Brigg. The site is made up of largely flat agricultural fields separated by hedgerows, trees and dykes. There is one farm access from Wrawby Road and a pedestrian access to the north, onto South View Avenue. The existing settlement of Brigg is to the west, with boundaries to the site being created by residential properties, roads and Brigg recreation ground. Adjacent to the recreation ground within the site is a line of poplar trees; open countryside is to the east with field boundaries to the eastern boundary. Adjacent to the northern boundary is open countryside and beyond the M180. To the south is residential development and open fields. A public right of way (PROW) (293) runs adjacent to the site along Brickyard Lane, parallel with the M180.

Constraints

The application site is mainly within the development boundary as defined by the Housing and Employment Land Allocations Development Plan Document (HELA DPD). The site is essentially within the BRIH-3 and BRIH-4 housing allocations in the council's HELA DPD, with some land proposed for open space/drainage within the open countryside.

The site is not within or adjacent to a conservation area, and there are no listed buildings within or adjacent to the application site. It does have potential for archaeological deposits.

The site is not designated as being of special ecological or landscape importance and is not immediately adjacent to any such designated area.

With regard to flood risk, the application site is entirely within flood zone 1 (low risk) on both the Environment Agency's flood maps and the Strategic Flood Risk Assessment (SFRA) for North and North East Lincolnshire.

There are no tree preservation orders on the site but there are trees on the site, some of which are of good amenity value, for example G73 (a line of poplar trees).

Relevant planning history

SCR/2022/9: EIA screening request for a residential development – determined not to be EIA development 11/10/2022.

PA/2023/1425: Outline planning application for residential development, with all matters reserved for subsequent consideration – pending.

(The site of PA/2023/1425 is adjacent to the site of this application, PA/2023/1236, within the BRIH-2 and BRIH-3 housing allocations in the council's HELA DPD, and has been made by the same developer as this application.)

The main issues to consider in the determination of this full application are whether the principle of residential development for 290 dwellings is acceptable and whether the proposal is acceptable in terms of highways, flood risk and drainage, heritage, ecology/biodiversity, design and impact on the character and appearance of the area, amenity, environmental matters, viability and associated S106 agreement.

Principle

Section 70 (20 of the Town and Country Planning Act 1990 and Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires planning applications to be determined in accordance with the development plan unless material considerations indicate otherwise. In this instance the development plan consists of the North Lincolnshire Local Plan (NLLP) which was adopted in May 2003, the North Lincolnshire Core Strategy (NLCS) which was adopted in June 2011 and the Housing and Employment Land Allocations Development Plan document (HELA DPD) which was adopted in March 2016. Material considerations exist in the form of national planning policy and guidance contained within the National Planning Policy Framework (NPPF) and the suite of documents comprising the Planning Practice Guidance (PPG).

The site is identified within the HELA DPD as within the housing allocations for Land at Wrawby Road Phase 2 (BRIH-3) for 333 dwellings and Land at Wrawby Road Phase 2 (BRIH-4) for 152 dwellings. Both these sites (BRIH-3 and BRIH-4) have similar site-specific criteria which include a mix of housing size and tenure, affordable housing provision, vehicular access point and improvements, good footpath and cycle provision throughout the site with linkages to local services, residential areas and rights of way network. A transport assessment and travel plan is required, a route is protected within the design of the site that maintains and safeguards an alignment for the creation of a single access link road to a suitable standard to connect Atherton Way and Wrawby Road, a flood risk assessment is required and where practical SuDS should be incorporated into the development, and significant landscaping to the boundary (BRIH-3). A heritage assessment and ecology survey are required, along with biodiversity enhancements, and a noise impact assessment is required. The design must accord with policy CS5 of the CS. The HELA DPD and policies BRIH-3 and BRIH-4 were adopted by North Lincolnshire Council in 2016.

Policy CS1 of the CS focuses on supporting market towns as thriving places to live, work and visit and as important service centres to serve the needs of local communities across North Lincolnshire. This includes providing important services for rural communities, an appropriate level and range of new housing development to support market towns as sustainable communities, and encouraging small and medium-scale employment opportunities to provide local jobs. The application comprises 290 dwellings including highways, open space, landscaping and drainage infrastructure for full planning permission. The proposed residential use is in line with the spatial strategy and is supported in principle by policy CS1 of the CS. Policy CS2 of the CS states, 'In supporting the delivery of the spatial strategy set out in policy CS1, as well as determining how future development needs will be met in North Lincolnshire, a sequential approach will be adopted. Development should be focused on:

1. previously developed land and buildings within the Scunthorpe urban area, followed by other suitable infill opportunities within the town, then by appropriate greenfield urban extensions;
2. previously developed land and buildings within the defined development limits of North Lincolnshire's market towns, followed by other suitable infill opportunities then appropriate small-scale greenfield extensions to meet identified local needs;
3. small-scale developments within the defined development limits of rural settlements to meet identified local needs.'

The application site is on a greenfield site (and is allocated for housing development in the Housing and Employment Land Allocations DPD (site references BRIH-3 and BRIH-4). The application site therefore conforms with policies CS2 and CS3 as the proposed residential properties are all within the defined development limits of Brigg, a market town. The principle of housing development in this location is therefore acceptable. Policy CS7 of the Core Strategy sets out an aspirational minimum density of 40 to 45 dwellings per hectare on sites within the market towns. However, this policy also states that, whilst housing developments should make efficient use of land, the density of new developments should be in keeping with the character of the area. Housing allocations BRIH-3 and BRIH-4 seek a density of approximately 40 dwellings per hectare. Brigg has a densely developed historic core; however, the application site lies on the outskirts of the settlement, where a less dense pattern of development is also evident. In addition, the red line boundary of the site extends out of the BRIH-3 housing allocation where the open space and drainage infrastructure (pumping station, storage tank and attenuation basin) are proposed to serve the development. The proposed development of 290 dwellings results in a density of approximately 37 net density per hectare, which falls below the minimum density sought by policy CS7, and BRIH-3 and BRIH-4. For the reasons set out above, it is considered that the proposed development of 290 dwellings, with a slightly lesser density than that sought by policy CS7, and BRIH-3 and BRIH-4, is appropriate in this instance and will make a sustainable use of the site whilst respecting the overall character of the area.

Policy RD2 of the NLLP sets out that development in the open countryside will be strictly controlled. This policy only supports residential development outside defined development limits in certain circumstances, none of which apply in this case. However, in this case no residential dwellings are proposed outside the allocation areas of BRIH-3 and BRIH-4. The red line site area extends eastwards outside BRIH-3, with this land proposed to be used for open space and drainage infrastructure to support the development. Policy RD2 does permit the provision of utility services within the open countryside. The applicant has stated that the drainage 'infrastructure has to be accommodated in this location in order to serve the development and cannot be accommodated elsewhere within the land allocated for housing due to the site topography and the drainage solution proposed.' The local planning authority considers that this justification is acceptable and therefore in this case the proposal will align with policy RD2.

The proposal conforms with policy CS8 as the Core Strategy focus for housing development is Scunthorpe and the market towns, which includes Brigg. The site is a greenfield site with the proposed dwellings lying within the development limits of the settlement (although some of the land, proposed for open space in connection with the proposed residential use, is outside the development limit). In this case, therefore, the proposal essentially conforms with this policy criteria.

The Government released an updated NPPF in December 2023. Paragraph 226 makes clear that for decision-making purposes only, certain local planning authorities will only be required to identify and update annually a supply of specific deliverable sites sufficient to provide a minimum of four years' worth of housing against the housing requirement (rather than five years). This includes authorities like North Lincolnshire, which have an emerging local plan that has been submitted for examination. These arrangements apply for a period of two years from the publication date of the revised NPPF. Therefore, full weight can be attributed to the Local Plan and Local Development Framework policies and the 'tilted balance' set out in paragraph 11(d) of the NPPF is not engaged.

The proposal is considered to represent sustainable development and broadly aligns with the strategic policies of the CS and the HELA DPD. The presumption in favour set out in paragraph 11(c) of the Framework applies. In determining the principle and sustainability of the proposed development, an assessment is required on the technical elements of the proposal which will be discussed below.

Highways

Policy CS25 of the CS and policies T1, T2, T6, T8 and T19 of the NLLP relate to highway matters, as does chapter 5 of the NPPF, and are therefore all relevant to this application.

The site is primarily within the development boundary of the market town of Brigg. It is approximately 1km from Brigg town centre where a full range of services, facilities and employment opportunities are available. The site is close to bus stops and adjacent to a cycle/pedestrian path. The village of Wrawby is also within walking distance. The site has good pedestrian/cycle and public transport links.

The A18 Wrawby Road is a single carriageway road which runs broadly north-east to south-west along the full application site frontage, where the carriageway is approximately 7.3m in width. It is bordered to the north-western side of the carriageway by a segregated footway/cycleway which varies in width but is typically around 2m wide, and is separated from the carriageway by a grass verge which itself varies in width between 2 and 3 metres along the site frontage. On its south-eastern side along part of the full application site frontage is a drainage ditch, which is separated from the carriageway by a grass verge, which generally varies in width between 2 and 3 metres.

A footway commences on the south-eastern side of the A18 Wrawby Road approximately 70m to the north-east of a priority junction with Churchill Avenue and continues to the south-west. Along the south-western section of the site frontage, it is subject to a speed limit of 30mph, which changes to 40mph approximately 70m to the north-east of the Churchill Avenue junction, where the footway commences.

Churchill Avenue forms the minor arm of a priority T-junction with the A18 Wrawby Road directly opposite the application site, approximately 70m to the north-east of the south-western full application site boundary. This junction incorporates a right-turn ghost island and advisory cycle lanes around the junction radii. Two crossing points are provided on the A18 to either side of the junction to facilitate cycle access between Churchill Avenue and the footway/cycleway provision on the north-western side of the A18 Wrawby Road. Churchill Avenue is a residential access road which provides access into the residential areas in the eastern extents of Brigg.

The proposed 290 dwellings will be accessed via a new priority T-junction with the A18 Wrawby Road located at the south-eastern site boundary, which will incorporate a right-turn ghost island facility. The proposed site access junction will be located approximately 215m to the north-east of the Churchill Avenue junction with the A18 Wrawby Road. The proposed site access junction for this application site will form the eastern end of the new link road, which will run through the application site broadly parallel to the eastern site boundary. The new link road will have a carriageway width of 6.7m, with a 2m wide footway on one side and a 2m wide verge and 3m wide shared footway/cycleway on the other side.

The proposed link road will run through the application site up to the north-western boundary with the outline application site (pending application PA/2023/1425), a distance of approximately 400m. In the short term, there will be minor access junctions on approach to the termination of the 6.75m wide access road, shown on the proposed site layout plan, which will serve as temporary turning facilities close to the end of the first phase of the provision of the link road.

The proposed site access includes visibility provision at the site egress, and the site access drawing shows that visibility splays of 4.5 m x 120m, suitable for the speed limit on the A18 Wrawby Road in the vicinity of the proposed site access of 40mph, are achievable in both directions. An emergency vehicular access will be provided at the proposed pedestrian access point to South View Avenue, which will be a minimum of 3.7m wide.

A separate 6m wide private drive access onto the A18 is proposed to serve three dwellings (plots 245 to 247) in the south-western corner of the site. The proposed private drive access will be formed through a vehicular dropped crossing arrangement, which will be similar to the other existing private drive accesses provided directly from the A18 Wrawby Road to the west of the site.

A 2m wide footway is proposed to the western side of the new link road as it passes through the full application site. A 3m wide shared footway/cycleway is proposed to its eastern side, which will tie in to the existing footway/cycleway on the north-western side of the A18 Wrawby Road. The proposed shared footway/cycleway will be separated from the carriageway by a 2m wide verge.

Two other pedestrian/cycle links will be provided as part of the application proposals. One will be provided onto the A18 Wrawby Road opposite the junction with Churchill Avenue and another will be provided directly onto South View Avenue. The South View Avenue pedestrian access will also provide emergency access to the application site for vehicles.

Each dwelling will have its own off-street car parking and electric vehicle charging point within the curtilage of each property as required under building regulations. The submitted swept analysis demonstrates that a refuse vehicle can enter and leave the site from the A18 Wrawby Road, at the proposed site access junction. Swept path analysis of the internal site layout, including turning heads, has been carried out which demonstrates that the site can be serviced as necessary, with all turning heads capable of allowing a refuse vehicle to turn around within the site and thereby enter and leave the site in a forward gear.

The council's Highways department has assessed the proposal with respect to its impact on highway safety. They have raised no objections with regard to the number of dwellings proposed or the ability of the local highway network to accommodate the potential additional vehicular and/or pedestrian traffic. Therefore, Highways have no objections to the proposed development subject to conditions which are all recommended below.

National Highways (NH) have also raised no objections to the proposals and are satisfied that the impact of development will not have a significant impact on the safe and efficient operation of the strategic road network, in this instance, the M180 at junctions 4 and 5. NH are requesting two planning conditions for the submission of a construction traffic management plan and for the submission of a boundary treatment plan for the M180 before development commences. It should be noted that procedurally, should the local planning authority not propose to determine the application in accordance with this recommendation, they are required to consult the Secretary of State for Transport and may not determine the application until the consultation process is complete. However, these conditions proposed by NH are recommended below and therefore there is no requirement to consult the Secretary of State in this regard.

For the reasons outlined above, it is considered, subject to the recommended highways conditions, that the proposed development would have no unacceptable impact on highway safety. In terms of highway matters, therefore, subject to conditions, the proposal aligns with chapter 5 of the NPPF, policy CS25 of the CS, policies T1, T2, T6, T8 and T19 of the NLLP, and the BRIH-3 and BRIH-4 housing allocations in the HELPA DPD.

Active Travel England (ATE)

ATE is responsible for making walking, wheeling and cycling the preferred choice for everyone to get around. ATE's objective is for 50% of trips in England's towns and cities to be walked, wheeled or cycled by 2030. ATE is an executive agency sponsored by the Department for Transport (DfT).

The Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2023 (DMPO) identified ATE as a statutory consultee for planning applications with effect from 1 June 2023. This means that a local planning authority is obliged, from 1 June 2023, to consult and consider comments made by ATE as set out in articles 18 and 19 of the Town and Country Planning (Development Management Procedure) (England) Order (2015).

ATE, in their response, have recommended deferral of the application, but if the council are minded to grant consent, ATE recommend a number of planning conditions. The developer has confirmed that they are willing to accept the conditions recommended by ATE in relation to the provision of a controlled crossing point on the spine road (link road), that suitable cycle parking is provided for each dwelling on the site and that the detail of site access arrangements (Wrawby Road and South View Avenue) is submitted for approval to the council. NLC Highways have also recommended these conditions and they are recommended below.

ATE have also recommended a condition in relation to the provision of a controlled pedestrian/cycle crossing on the A18 Wrawby Road outside the application site at its junction with Churchill Avenue. ATE have suggested that this is required to provide connections to the residential areas to the south and for people to cross the A18 to access the existing bus stops. Whilst this may be desirable by ATE, there are already two existing uncontrolled pedestrian/cycle crossing points in place on the A18 Wrawby Road, either side of the junction with Churchill Avenue. These include refuge islands (to allow pedestrians and cyclists to cross the A18 in two discrete movements), dropped kerbs and tactile paving, and these crossings will be retained as part of the development proposals.

The existing crossing points will remain appropriate to serve the proposed development site, and the likely increases in crossing movements that will result as a consequence of the development. The most convenient pedestrian and cycle routes between the site and the facilities in the town centre of Brigg (to the south-west of the site) would be via the footway/cycleway provision on the north-western (site) side of the A18 Wrawby Road, which would not necessitate residents of the site having to cross the A18 Wrawby Road. There are few facilities on the south side of the A18 in the vicinity of Churchill Avenue which would require pedestrians from the proposed development site to cross the A18 in this location.

An existing signalised pedestrian crossing point is in place on the A18 Barnard Avenue around 100m to the north-west of Monument Roundabout, which facilitates access to the amenities in the town centre of Brigg to the south of the A18. This existing crossing is on the desire line between the proposed development site and the town centre, and is considered to be in an appropriate location to serve the demand. It is not considered likely or necessary that future residents of the proposed development site will wish to cross the A18 at Churchill Avenue when travelling to and from the centre of Brigg on foot or by bicycle.

The personal injury collision data does not include any personal injury collisions at either of the two existing crossing points during the 81 month period from 1 January 2016 to 29 September 2022. No existing highway safety issue associated with the use of the existing crossing points has been raised in the submitted Transport Assessment or by NLC Highways. The existing uncontrolled crossing points on either side of Churchill Avenue provide a suitable crossing facility between the site and the south side of the A18 Wrawby Road, including the existing westbound bus stop on the A18. The existing uncontrolled pedestrian/cycle crossing points with refuge islands, to either side of Churchill Avenue, are suitable for accommodating crossing movements in this location, and will be able to accommodate the increases in pedestrian and cycle movements which are likely to be generated with the proposed development in place. As such, the evidence base has demonstrated that it is not considered that providing a controlled parallel crossing over the A18 is required to serve the proposed development or to mitigate impacts of the development. Therefore to impose such a planning condition would fail to meet all the six tests for planning conditions as set out in the PPG and, as such, a condition is not considered to be necessary, nor it is relevant to the development proposed to be permitted and would not be reasonable. As a result, no condition recommending a controlled crossing on Wrawby Road is proposed.

The proposed development, subject to conditions, is considered to be acceptable in terms of active travel and therefore aligns with chapter 9 of the NPPF, policy CS25 of the CS, policies T1, T2, T6 and T8 of the NLLP, and the BRIH-3 and BRIH-4 housing allocations in the HELPA DPD.

Public right of way (PROW)

The site lies close to footpath 293 (Brickyard Lane) and the development proposal will change the views from the PROW. Currently, there are views of open fields from this PROW and the proposal will change this view to a residential housing estate with open space to the eastern side of the site. This will be discussed in more detail in the landscape section of this report. Comments from the PROW officer are noted. The applicant does not own the land between the eastern boundary of the site and Brickyard Lane and therefore a connection between the application site and footpath 293 cannot be made to improve

connectivity to the open countryside. Occupiers of the proposed development will still be able to access footpath 293 by leaving the development onto Wrawby Road and utilising the footway on Wrawby Road to connect to footpath 293 (Brickyard Lane).

There may also be an opportunity to provide a pedestrian link from footpath 293 or footpath 284 into the site for pending application PA/2023/1425. PA/2023/1425 shares a boundary with footpaths 284 and 293. This matter will be addressed within the report for PA/2023/1425 which is currently pending consideration.

In terms of paragraph 104 of the NPPF and policy CS25 of the CS, the proposed development still provides the opportunity for occupiers of the development to connect to the adjacent PROW using the footway along Wrawby Road. Potentially, there is an opportunity, which will be fully explored, to provide a pedestrian connection to the adjacent PROW within application PA/2023/1425, which is currently under negotiation with the applicant.

Flood risk and drainage

Policies CS19 of the CS and DS16 of the NLLP relate to flood risk; policies CS18 and CS19 of the CS, and DS14 of the NLLP, relate to drainage. The NPPF (section 14) is also relevant. A flood risk and drainage assessment has been submitted with the application to assess the development's risk from flooding and the suitability of the site in terms of drainage. In addition, documents have been submitted in relation to SuDS. The LLFA, EA, Anglian Water (AW) and internal drainage boards have all been consulted on the proposals.

Flood risk

In terms of flood risk, the site is in flood zone 1 of the council's SFRA and is therefore at low risk of flooding. The flood risk assessment has considered all risks of flooding from fluvial flooding, surface water flooding, flooding from open drainage ditches, groundwater flooding, flood risk from existing water mains, flood risk from existing drainage/sewers, flood risk from proposed drainage/sewers, flooding from reservoirs, canals and other artificial sources. The report has concluded that there is no potential risk of flooding to the development and therefore there is no requirement for any flood mitigation measures. The LLFA, EA (Environment Agency), AW and internal drainage boards (IDBs) are not objecting to the proposals in terms of flood risk and are not recommending planning conditions in that regard. In terms of flood risk, therefore, the proposal aligns with the NPPF, policy CS19 of the CS and policy DS16 of the NLLP.

Surface water drainage and SuDS

An extensive drainage strategy and associated documents, including a justification for SuDS on the site, has been submitted as part of the application. In terms of surface water, it is proposed to discharge the development surface water to the existing watercourse network crossing the site, with the primary watercourse consisting of Briggfield Drain. Due to the existing topography and depth of the watercourses, it is necessary to pump surface water run-off to the outfall. Part of the development proposal comprises a pumping station on the application site. Development run-off flows are to be restricted in accordance with existing greenfield run-off rates based upon the requirements of the local Internal Drainage Board (Shiregroup), which are 1.4l/s/ha with a total maximum discharge to watercourse of 16.3l/s. Based on this discharge rate, the attenuation required for the development is 4,285m³ (1 in 100 year event + 40% c/c). Due to the elevated groundwater table, it is

proposed to attenuate the 1 in 30 year event flows in a below ground tank (2,125m³) with up to the 1 in 100 year event (+ climate change) flows attenuated in a detention basin (2,160m³) In addition to the attenuation tank and basin, it is proposed to install a wet pond feature for SuDS.

SuDS have been fully considered and assessed in various documents submitted with the application. The following SuDS components are proposed for the development site:

Rainwater harvesting water: Butts provided on each dwelling, reducing the amount of water entering the drainage system (quantity) and providing a water source to each homeowner.

Attenuation tank: Underground tank to store the water during heavy rainfall (quantity), with a slow release to the watercourse to ensure no increased flood risk. Location of tank within a large green open space providing and large usable space for the public (amenity).

Detention basin dry: Local depression in ground providing an area for water storage during heavy rainfall (quantity) which is to be used in conjunction with the attenuation tank. The detention basin provides supplementary benefit of treating the run-off (quality) and, with the proposed planting, increased habitat in the open space (amenity and biodiversity).

Pond: The wet planted pond area within the open space provides a diverse habitat (biodiversity) and an enhancement to the space (amenity), whilst providing all the treatment requirements (quality) of the surface water before discharge to the watercourse.

The submitted SuDS reports concludes that, 'The proposed development has been assessed in accordance with North Lincolnshire Council SuDS and Flood Risk Guidance and the Ciria SuDS Manual (C753), 2015. A combination of SuDS techniques have been incorporated into the proposed development ensuring there is no increased flood risk created by the proposed scheme.'

In terms of surface water disposal, Anglian Water have commented that, '*the preferred method of surface water disposal would be to a sustainable drainage system (SuDS) with connection to sewer seen as the last option. From the details submitted to support the planning application the proposed method of surface water management does not relate to Anglian Water operated assets. As such, we are unable to provide comments on the suitability of the surface water management.*' As a result, Anglian water are not proposing any planning conditions relating to surface water disposal.

The Shire Group have commented that they have no objections if surface water is disposed of by a soakaway system or is directed to a main sewer system. If surface water is to be discharged into an ordinary watercourse then consent from the IDB would be required and restricted to 1.4 litres per second per hectare or greenfield run-off. No obstructions within 9m from the edge of an ordinary watercourse are permitted without consent from the IDB. The submitted drainage information confirms that the greenfield run-off for the site has been calculated below in accordance with the IDB requirements of 1.40l/s/ha resulting in the development discharge to be restricted to 16.3l/s and there will be no obstructions within 9m of the watercourse. The IDB are requesting a condition to ensure that the restricted flow measures or attenuation are put in place before occupancy and within three months of development progressing on site. In this case the development is likely to be phased. The approved drainage scheme for any phase will need to be implemented on the site before any dwelling is occupied within any phase. This matter can be dealt with by planning conditions.

The LLFA has been consulted on the proposal and has no objections subject to conditions. The proposal, in terms of surface water drainage and SuDS, subject to the recommended planning conditions, is considered to be acceptable and aligns with policies CS18 and CS19 of the CS, policy DS14 of the NLLP, paragraphs 165 to 175 of the NPPF, and the BRIH-3 and BRIH-4 housing allocations in the HELA DPD.

Foul water

The applicant's FRA states that in order to accommodate the proposed development, it is necessary to divert the existing combined sewer which crosses the site. This will be undertaken under a Section 185 Agreement with Anglian Water. It is proposed to discharge the foul water flows from the development to the diverted combined water sewer. Due to the topography of the site and the depth of the existing sewer network it is proposed to pump foul water flows. The foul and surface water drainage networks are to be adopted by Anglian Water or an Independent water authority under a Section 104 Agreement. Therefore, the maintenance of the networks is the responsibility of Anglian Water or an independent water authority.

Anglian Water originally had no comments to make on the application. However, they have been consulted on the amendments, including the drainage information and updated FRA submitted in March 2024, and have responded to this consultation. They have confirmed that the foul drainage from this development is in the catchment of Brigg Water Recycling Centre that will have available capacity for these flows. The proposed connection in the FRA March 2024 is acceptable in principle although there are capacity constraints within the network. Anglian Water have stated that, *'Consequently the full development may lead to an unacceptable risk of flooding and/or pollution. Anglian Water will need to plan effectively for the proposed development, if permission is granted, and will need to work with the applicant to ensure any infrastructure improvements are delivered in line with the development.'* Anglian Water are therefore recommending conditions requiring a phasing plan and/or an on-site drainage strategy and informatives concerning the sewer arrangements for the site.

With regard to the used water sewage network, Anglian Water are recommending conditions requiring a phasing plan to be submitted to the council for the development along with conditions relating to a scheme for on-site foul water drainage works, including connection point and discharge rate, to be submitted to and approved in writing by the local planning authority, and prior to the occupation of any phase, the foul water drainage works relating to that phase must have been carried out in complete accordance with the approved scheme.

To ensure that satisfactory foul water drainage can be achieved on the site, the aforementioned planning conditions are recommended. Anglian Water will be consulted on any phasing plan/foul water details submitted. Therefore, subject to the recommended conditions, the proposal would align with policy DS14 of the NLLP.

Heritage – archaeology

Policy HE9 of the NLLP relates to archaeological evaluation and policy CS6 of the CS relates to the historic environment. Chapter 16 of the NPPF, relating to conserving and enhancing the historic environment, is also relevant. The housing allocations BRIH-3 and BRIH-4 both require a heritage statement to be submitted to demonstrate that the

development will have no adverse impact on the historic environment. The site is not within or adjacent to a conservation area, nor are there any listed buildings on, adjacent or close to the application site. The council's Historic Environment Record (HER) have been consulted on the application.

The site does have potential for archaeology based on previous records. On the southern area of the site previous archaeological investigations have been carried out. An archaeological and heritage desk-based assessment has been submitted that identifies the potential of the site to contain archaeological remains of prehistoric and/or Roman date. There is the potential that any below-ground construction works in this area could encounter significant and well-preserved archaeological remains relating to early occupation and activity of prehistoric and/or Roman date that is attested in the record as present across the surroundings. The groundworks associated with a residential development would be likely to result in the disturbance and destruction of any archaeological evidence. Because the site may contain heritage assets of archaeological interest, adequate information is required about the significance of any such assets to properly assess the impact of the proposed development, and thereby to inform the decision-making process in accordance with the NPPF and local planning policies (mentioned above).

As a result, the submitted archaeological and heritage desk-based assessment recommended that archaeological field evaluation in the northern area of the site was required to assess the archaeological interest and significance of the whole site. The results of the field evaluation will inform the assessment of significance and the impact of the proposals, as well as what mitigation measures may be appropriate, and should be provided in the application. The NPPF (paragraphs 200 and 201) makes it clear that identification and assessment of the significance of heritage assets is required prior to the determination of an application. HER, who have been consulted on the application, recommended that archaeological field evaluation needed to be carried out on the site which comprised of fieldwalking and geophysical surveys, followed by trial trenching.

Following the comments and recommendation made by HER, the applicant has carried out and submitted the required archaeological field evaluation to the council. HER have been consulted on the submitted reports which have ascertained that no significant archaeological features have been identified across the application site and the residual potential for the presence of as yet unrecorded archaeological remains is judged to be low. Therefore, no further archaeological work in advance or during the proposed development is necessary and HER are not recommending any archaeological conditions if planning permission is granted.

To conclude, the site does not contain any significant archaeological features. All the required archaeological investigations and fieldwork have been submitted to the council in accordance with policies HE9 of the NLLP, policy CS6 of CS, paragraphs 200 to 201 of the NPPF, and BRIH-3 and BRIH-4. No further work or planning conditions in respect of archaeology are required for this application.

Landscape

The applicant has submitted a landscape and visual impact assessment that is compatible with the Guidelines for Landscape and Visual Impact Assessment 3rd Edition (GLVIA3, 2013), produced by the Landscape Institute and the Institute of Environmental Management & Assessment.

The site is not covered by any formal landscape designations. The site does lie within the proposed extension to the Lincolnshire Wolds Area of Outstanding Natural Beauty (AONB) (new local plan policy DQE1) although limited weight can be given to this policy at present.

Policy LC15-11 seeks to create new and informal landscape areas, wildlife habitats and improved surroundings and to screen visually intrusive developments. The landscape enhancement schemes under policy LC15 are shown on the proposals map in an indicative manner. The actual scheme will be defined by a process of negotiation, The implementation and maintenance of allocated landscaping schemes will be achieved through the use of planning conditions and agreements.

The Adopted Landscape Assessment and Guidelines document (SPG5) gives the following guidance for this area: Flat Valley Bottom Farmland – Roxby, Bonby, Appleby, Worlaby Carrs, Cadney, Froghall, North Kelsey Carrs.

Landscape Strategy: Enhance this area by seeking to initiate measures that introduce more woodland, effectively manage those present and provide more scope for habitat creation through waterside planting and ditch re-modelling. Hedgerows where present require effective management and repair.

Landscape Guidelines: Encourage the planting of more mixed deciduous woodland in strategic areas that will not compromise the essentially open character, such as:

- a) a naturalistic distribution along watercourses. Water-tolerant species such as willow and alder to re-create carr-type vegetation;
- b) medium-sized woodland blocks concentrated near to areas with intrusive impacts, i.e. transport corridors and transmission lines, associated with farmsteads or linked to existing woodland blocks or hedgerows, discontinuous in nature.

Seek to increase the number of isolated roadside trees, particularly surrounding traditional villages.

Management and replacement incentives should be encouraged to develop the presence of hedgerows in the scenery, maintaining and infilling remnants. Give priority to hedgerows that follow the line of ditches to enhance their visual and ecological potential.

Seek to increase the cover of hedgerow trees near transmission lines where extending existing woodland would be unachievable or unsuited to the local scene.

Necessary new roads or improvements should seek to reflect the distinctive pattern of existing roads and lanes in the design of new alignments. The distinctive road pattern and pattern of small lanes should be protected with proposals to develop alternative crossings across the valley resisted.

Public rights of way through the valley should be maintained and in places enhanced to encourage improved low-key recreational access, such as footpaths, cycle routes and bridleways. Priority should be given to those rights of way that cross the valley, particularly where they offer connection opportunities onto the Lincolnshire Wolds escarpment.

The applicant's own landscape and visual impact appraisal also concludes that 'the long-term landscape and visual effects of the development are unlikely to exceed moderate adverse. The proposed scheme forms part of a wider allocation and will provide a

landscape framework that overtime softens and contains the settlement fringe, adhering to the Flat Valley Bottom Farmland LCT landscape guidelines. The Landscape Masterplan has considered the wider context of the allocation and defined an effective framework through which the effects of development as a whole can be mitigated. The levels of effect likely to arise from the proposal are common for greenfield development and consistent with the effects recorded for recent developments recently granted planning permission within the district.'

The proposals will have a significant effect on the landscape as arable agricultural land will be changed to a residential housing estate. The impacts of the development will be most keenly experienced by users of Public Footpath Wrawby 293 and by users of the main road (A18) approaching Brigg from Wrawby. These impacts must be balanced against the BRIH housing allocations and the significant landscaping and open space providing on the site to mitigate landscape impacts. Over time, landscaping and biodiversity net gain proposals will help to assimilate the development into the landscape. However, realistically, it could take 10 years or more for the landscaping to have significant beneficial effects. The landscaping scheme is acceptable and is consistent with the adopted landscape assessment and guidelines, and biodiversity net gain requirements. The precise landscaping scheme can be conditioned and will need to be based upon the submitted landscape master plan. Overall the proposal does align with policies CS5, CS16 and CS17 of the NLLP, policies DS1, LC15-11, H5 and RD2 of the NLLP and SPG5, and the BRIH-3 and BRIH-4 housing allocations in the HELPA DPD.

Ecology/biodiversity

Chapter 15 of the NPPF, policies CS5 and CS17 of the CS, and policies LC5, LC6 and LC12 of the NLLP relate to ecological matters. BRIH-3 and BRIH-4 both require an ecology survey and biodiversity enhancements should be incorporated into site design. A preliminary ecological appraisal, water vole survey, bat activity survey, and BNG metric have been submitted with the application. The council's ecologist has been consulted on the proposals.

In terms of protected species, the survey methods used and the survey effort deployed by the developer's consultants are appropriate for the site in question. The surveyors recorded low levels of bat activity, comprising three common and widespread bat species. No evidence of water vole activity was recorded. The site has some potential for nesting birds, hedgehogs and badgers, but is not likely to support reptiles or great crested newts. Standard sensitive working methods will be required for protected and priority species which can be secured by planning conditions.

The application site is largely arable farmland, but does have biodiversity value in marginal hedgerows and drains and the M180 margins. Whilst the proposal will affect a very large area, the habitats and species affected are widespread and commonplace. In order for the proposal to align with paragraphs 180 and 186 of the NPPF, policies CS5 and CS17 of the CS, and policies LC5, LC6 and LC12 of the NLLP, it will be necessary to impose planning conditions to secure biodiversity enhancements on the site. This will include a condition requiring a biodiversity management plan (BMP) to be submitted to the council for approval. This will ensure bat and bird boxes are installed on the site, the installation of sparrow and swift boxes and terraces, provision for hedgehogs to pass through installed fencing, and details of wetland habitat to be created and managed as part of sustainable drainage, for example.

In terms of biodiversity net gain (BNG), the application was submitted before 12/02/2024 (when it became mandatory for developers to deliver a BNG of 10%) and therefore for this application only a BNG net gain has to be achieved on the site – this can be a 1% BNG. The developer has submitted an initial and then a revised Biodiversity Metric 3.0 assessment for the proposal. The latest BNG metric reveals:

- an increase in habitat units of 3.24 (10.03%)
- an increase in hedgerow units of 2.11 (20.83%)
- an increase in river units of 0.19 (12.08%).

The developer considers that there will be a 10.03% BNG on the site. The council's ecologist does not consider the BNG is as large as 10% but accepts a BNG can be provided on the site and the submitted information does indicate BNG can be achieved on the site. The BMP condition will ensure that at least a 1% BNG is achieved on the site. Therefore, subject to this condition, the proposal will align with chapter 15 of the NPPF, policies CS5 and CS17 of the CS, policies LC5, LC6 and LC12 of the NLLP, and BRIH-3 and BRIH-4 of the HELA DPD.

Trees

The submission included landscape plans for the full part of the proposal, a tree survey of existing trees and an arboriculture report. The submitted BNG report is also relevant. Policies CS5 and CS17 of the CS, and LC12 of the NLLP, specifically relate to trees. Within the BRIH-3 and 4 housing allocation is a landscape enhancement scheme, LC15-11 (Wrawby Road Approach), which runs along the western boundary of the allocation and part of the southern boundary of these allocations. The BRIH-3 housing allocation in the HELA DPD specifies that significant landscaping will need to be provided around the boundary in order to mitigate the visual impact that this proposal will create.

The developer has submitted various documents relating to trees and landscaping for the site. In terms of trees, the developer has submitted a document clearly stating which existing trees are to be removed to facilitate the development. Concerns from objectors about the loss of these poplar trees are noted, together with comments about the development being vulnerable to ball strikes and loss of a dog walking facility. The developer has confirmed that these poplar trees will remain, with a small number of them removed to facilitate the site access or that have been categorised as 'U' in the submitted tree report.

Turning first to the loss of trees on the site, none of the trees on the site are protected as none are within a conservation area or covered by a tree preservation order (TPO). Therefore, these trees can be felled without any consent from the council. The poplar trees on the western boundary are a feature on the landscape and currently form a distinct boundary between the site and the urban area of Brigg, and are shown to be retained on all the submitted plans. Tree protection conditions are recommended to ensure the longevity of these trees. With regard to potential cricket ball and football strikes onto the site from the recreation ground, there is over 20m between the dwellings and the boundary with the recreation ground, with the poplar trees in between. The 'threat' of ball strikes is limited and the retention of the poplar trees, proposed landscaping and the proposed boundary treatment on site frontages will also mitigate ball strikes to a certain extent if these occurred.

A small number of existing trees will be felled on this site; these are mainly category 'C'. However, the developer has provided a landscape masterplan showing significant landscaping on the site to mitigate the loss of existing trees. There will be a significant landscape buffer along the western boundary of the site, with the retention of poplar trees and planting, good tree planting within the site creating tree lined streets, as required by the NPPF (chapter 12 paragraph 136), and areas of open space within the development and to the eastern edge where the drainage infrastructure will be sited and landscaping along the drainage easements. The attenuation pond will also provide opportunities for landscaping. Existing hedgerows will be retained and enhanced and additional hedgerows and shrub planting is proposed within the main development. Neutral grassland to the eastern side of the site is proposed. On the site frontage adjacent to Wrawby Road, new trees are proposed and green areas. The landscaping can be conditioned so that it is in accordance with the masterplan, but the exact nature of the trees, hedges, grass mix, new and enhanced hedgerows and mix is submitted to the council for approval to secure the robust landscaping and tree planting scheme proposed within the masterplan and any losses are replaced. Subject to the aforementioned conditions, the proposal will align with the NPPF, policies CS5 and CS17 of the CS, policies LC12 and LC15-11 of the NLLP, and the BRIH-3 allocation.

Design and impact on the character and appearance of the area

BRIH-3 and BRIH-4 both require a mix of housing size and tenure on the site. Two hundred and ninety dwellings are proposed on the site with a mix of two-, three- and four-bedroom dwellings. The dwellings will be semi-detached and detached two-storey dwellings apart from the four proposed bungalows. There are 15 house types proposed on the site. The gross housing density for the site is 26dph, the net developable area creating a net housing density of 37dph. This is slightly below the density required in the BRIH-3 and BRIH-4 allocations, but it is accepted that, due to the topography of the site, significant areas of green open space, and provision of semi and detached dwellings, and the bungalows, does justify a lower housing density than that required by BRIH-3 and BRIH-4.

A palette of materials is proposed with a predominance of red brick, which is prevalent in this area of Brigg. All dwellings will each have two off-street car parking spaces. Drainage infrastructure is proposed on an area of the site which lies within the open countryside. This drainage infrastructure comprises a pumping station, storage tank, attenuation basin and pond. Overall, the scheme will provide around 0.6 hectares of public accessible space. Significant landscaping is also proposed on the site, along with the line of poplar trees on the western boundary to remain in situ, forming a green wedge to the western boundary and retaining this important landscape feature for future generations.

The main access to site is from Wrawby Road. Here, an active, forward-facing development along Wrawby Road has created a strong street scene. Different housing types and differing materials and detailing across these frontage plots add to the vitality of the development. Homes are served by private drives with no direct vehicle access. A green corridor along the site frontage softens the development and adds to the rural character of this area of Brigg, whilst reducing road traffic noise from the A18. Dwellings are aligned with views along the streets creating natural surveillance. Within the residential development are significant green areas and landscaping that further soften the development and will assist with the simulation of development into the landscape.

The dwellings have good spacing between them and well-sized garden areas. There is clear definition between the public and private domain with all properties having 'defensible

spaces' to their frontages, and sides on a corner position, with a variety of hard and soft treatments to the street, some giving physical enclosure to the semiprivate spaces such as railings, hedges and shrub beds. Boundary treatments to rear gardens will safeguard privacy. The local vernacular has influenced the appearance of the new homes. The buildings include large windows with a horizontal emphasis, consistent eaves and ridge lines and a well-proportioned built form.

The housing mix and house types comprise two-, three- and four-bedroom properties (semi-detached and detached dwellings) and four bungalows. Strategic Housing have confirmed that the housing mix for the site is acceptable and aligns with the Housing and Economic Needs Assessment (HENA).

Therefore, in terms of density, design, housing mix, house type and layout, the development is considered to be in character with the market town of Brigg and will therefore align with policies CS5, CS7 and CS8, and the housing allocations of BRIH-3 and BRIH-4.

Amenity

In terms of amenity, the design and layout of the dwellings will not result in any demonstrable harm to the amenity of occupiers of the development. Each dwelling has adequate private amenity space and adequate separation distances between dwellings, and window openings are positioned to avoid overlooking. Planning conditions will be used to ensure all en-suite, WC and bathroom windows will be obscure glazed to safeguard privacy.

In terms of existing adjoining properties, to the south are properties along Wrawby Road and Churchill Avenue. These properties are on the southern side of Wrawby Road. The proposed dwellings on the site frontage (facing Wrawby Road) are all set back from Wrawby Road with a corridor of green space in front of these dwellings and the access/driveways to these properties. There is a minimum distance between the proposed dwellings on the site frontage and those existing dwellings on Wrawby Road/Churchill Avenue of 40m. All the proposed dwellings will be two-storey apart from the four bungalows. Due to the siting of the proposed dwellings and separation distances, there will be no loss of residential amenity to the existing dwellings to the south. It is accepted that, visually, the view of the site for existing residents will change from that of open agricultural land to a residential housing estate. However, the site has been allocated for residential development since 2016 and although, visually, the site will change, there will be no overlooking/loss of privacy or overbearing issues to the existing residents to the south on Wrawby Road and Churchill Avenue.

To the west are properties on Wrawby Road and the recreation ground. The nearest property to the west is 33 Wrawby Road, a large detached dwelling. There are various hedges, trees and boundary trees on/close to the boundary of the site with 33 Wrawby Road. Plot 247 is approximately 12m from 33 Wrawby Road. Plot 247 is a detached dwelling with no window openings in the side elevations. Plots 239 and 240 are set behind plot 247 and are a pair of semis. Plots 239 and 240 are two-storey dwellings adjacent to the rear garden of 33 Wrawby Road. The only opening in the side elevation is a landing window which is not a habitable room. Therefore, the positioning of these plots will not result in any demonstrable loss of amenity to 33 Wrawby Road.

The rear gardens of plots 231 to 235 adjoin the rear garden boundary of 31 and 33 Wrawby Road. Due to separation distances in excess of 40m between these plots and 31 and 33 Wrawby Road, there will be no direct overlooking into the private garden areas of these properties. The rest of the plots to the western boundary face the recreation ground.

To part of the north and western boundaries are properties on Horstead Avenue and Foxton Way. A new development of bungalows is currently under construction on the former Brigg Resource Centre. The rear gardens of plots 176 to 190 are located along the northern boundary with Horstead Avenue. The siting and separation distances of these plots to the existing dwellings and the bungalows under construction will not result in loss of privacy/overlooking or overbearing issues to these properties. Plots 135, 108 and 101 to 107 are close to the western boundary. Also along this boundary is a wide corridor of green space. There are substantial separation distances between these plots and properties on Horstead Avenue and Foxton Way. It is noted that the first and second floor flats on Foxton Way will have a view of some of these plots. However, these flats are over 28m from the plots so any overlooking will be marginal, the flats looking over the front garden area and parking spaces of these plots.

Plots 71, 72 and 78 to 83 are sited along the northern boundary, adjacent to agricultural land. To the eastern boundary is agricultural land and beyond Brickyard Lane there are no existing residential properties close to the application site on these boundaries.

The proposal, in terms of amenity, would align with policies CS5 of the CS, H5 and DS1 of the NLLP, and the BRIH-3 and BRIH-4 housing allocations in the HELPA DPD.

Environmental matters

Contamination

Policy DS7 of the North Lincolnshire Local Plan is concerned with contaminated land. BRIH-3 housing allocation requires a land contamination assessment to be carried out so that appropriate design criteria and mitigation measures can be considered. The site currently comprises five agricultural fields which are separated by drainage ditches, hedges and trees. The site is approximately 24ha in size. The historical maps dating back to 1886 identify that the site has been used as agricultural land and has not been subjected to any former industrial land use; however, arable farming activities may have given rise to some potential contamination on site. A preliminary geoenvironmental investigation has been submitted by the developer. Environmental Protection have been consulted on this document. The preliminary conceptual site model has identified several potential contaminants associated with different farming activities over the years which may give rise to contamination of the soils. As the proposed end use is residential, it is of a sensitive nature, and the report recommends an intrusive investigation is required. With this in mind, Environmental Protection are recommending a full contamination condition. Subject to the imposition of the condition outlined above, the proposal is acceptable with regard to contamination and would align with policy DS7 of the NLLP and the BRIH-3 residential allocation in the HELA DPD.

Air quality

Policy CS18 of the CS is concerned with sustainable resource use and climate change. Its purpose is to promote development that utilises natural resources as efficiently and sustainably as possible. Two points of this policy are relevant: (10) ensuring development

and land use helps to protect people and the environment from unsafe, unhealthy and polluted environments, by protecting and improving the quality of the air, land and water; and (12) supporting new technology and development for carbon capture and the best available clean and efficient energy technology, particularly in relation to the heavy industrial users in North Lincolnshire, to help reduce CO2 emissions.

An air quality assessment has been submitted with the application which considers the air quality impacts from the construction phase and operational phase, and the suitability of the site for residential development. The conclusion of this assessment is that the potential risk of dust soiling is high from earthworks, construction and trackout activities. The potential risk of human health impacts is medium from earthworks, construction and trackout activities. To mitigate these impacts, Environmental Protection recommend restrictions on the working hours for construction, demolition and site clearance operations, and the submission of a construction environmental management plan (CEMP) for approval to the council.

In terms of the operational phase of the development, the submitted assessment provides details of impacts from the traffic generated by the development on the local area. An ADMS-Roads dispersion mode has been used to predict the air quality impacts from changes in traffic on the local road network from the following pollutants: PM10, PM2.5 and NO2. Following comments from Environmental Protection on the operation phase impacts, the developer has submitted additional information to the council. This information comprises an updated air quality assessment and an email response to the questions raised by Environmental Protection. Environmental Protection have assessed this updated/additional information and are satisfied that the development is acceptable in terms of air quality. The proposal therefore aligns with policy CS18 of the CS in terms of air quality.

Noise

Policies DS1 and DS11 of the NLLP relate to noise, along with policy CS5 of the CS. BRIH-3 housing allocation requires a noise assessment to be carried out so that appropriate design criteria and mitigation measures can be considered. Environmental Protection have been consulted on the proposals. It is acknowledged that some noise and disturbance will be experienced during the construction of the development and that, due to the scale of the proposed development, this could take place over a significant period of time. However, this type of disturbance is generated by almost all new developments and is limited to the construction phase only. The council's Environmental Protection officer has recommended conditions restricting hours of construction operations and to secure an acceptable construction environmental management plan providing mitigation measures in relation to noise, vibration, dust and lighting during demolition and construction operations. It is considered that the recommended conditions will mitigate the impacts of the construction phase on neighbouring properties to an acceptable level.

The main source of noise is from road traffic along Wrawby Road and from the M180. An environmental noise survey at the site was undertaken between 8 to 10 June 2022. Measurements were collected at two unattended measurement locations; the dominant noise source at the site was established as being road traffic noise from the M180 and Wrawby Road. The report has assessed the site using appropriate guidance, Professional Practice Guidance on Planning and Noise for new residential development (ProPG) and British Standard BS8233:2014. The initial ProPG Stage 1 Assessment finds that acoustic challenges at the site are of low to medium risk.

The ProPG Stage 2 Assessment reports that the site is located a large distance from the M180 with future residential premises expected to be present in the future within the proposed development site to the north (subject to a separate planning application PA/2023/1425). This would result in reduced noise levels in the northern part of the site due to screening from the M180. The report includes an Acoustic Design Statement and the design of the site includes a stand-off to Wrawby Road with the first row of buildings facing Wrawby Road orientated so that the buildings provide a barrier to rear gardens and dwellings located further into the site.

To meet internal noise levels, a glazing and ventilator strategy has been identified to meet the required BS./who criteria for internal spaces. Sound insulation can be conditioned. External noise levels are predicted to be below the BS 8233 guideline value in all private gardens. Therefore, Environmental Protection are proposing planning conditions requiring the submission of a noise mitigation scheme to be approved by the council, and a verification report to be submitted for approval to the council, before the dwellings are occupied, which demonstrates the effectiveness of the mitigation measures. Subject to the aforementioned conditions, the proposal is considered to be acceptable in terms of noise. In terms of the noise impacts in this regard, the proposal would align with paragraph 193 of the NPPF, policy CS5 of the CS, policies DS1 and DS11 of the NLLP, and the BRIH-3 housing allocation in the HELA DPD.

Section 106 agreement and viability

Policy CS27 is concerned with planning obligations and states that where a development proposal generates an identified need for additional infrastructure, North Lincolnshire Council will, through the negotiation of planning obligations pursuant to Section 106 of the Town and Country Planning Act 1990 and in accordance with guidance set out in Circular 05/2005, seek obligations that are necessary to make proposals acceptable in planning terms. The tests for planning obligations are set out in Part 11, section 122 of the Community Infrastructure Levy Regulations 2010 (as amended). It states: (2) A planning obligation may only constitute a reason for granting planning permission for the development if the obligation is: (a) necessary to make the development acceptable in planning terms; (b) directly related to the development; and (c) fairly and reasonably related in scale and kind to the development. The legal test is also set out in planning policy under paragraph 57 of the National Planning Policy Framework 2023.

Affordable housing

Policy CS9 is concerned with affordable housing and requires schemes for new residential housing in market towns to contribute 20% of the scheme for affordable tenure. Ideally 70% of affordable housing supplied should be for rent and, where possible, the housing should be provided on site, but an off-site contribution may be acceptable in certain circumstances. Under policy CS9, the site requires 20% of the total number of dwellings to be provided on the site to be affordable. Based on 290 dwellings, this will equate to up to 58 affordable dwellings on the site.

Education

SPG8 Developer Contributions to Schools and policy C1 of the NLLP is relevant. Based on current 2023/24 figures, the education contribution for development in Brigg is, as amount per dwelling: for primary, £4,807 and for secondary, £5,795. Therefore, the total education

contribution per market dwelling amounts to £10,602 (affordable dwellings are excluded from an education payment).

Recreation

Policy CS23 of the CS requires contributions from developers towards recreational and leisure facility improvements, towards necessary improvements or additional provision for recreation facilities and open space arising from their development proposals. A total contribution of £246,030 is required for recreation which is equivalent to £848.38 per dwelling. the breakdown of this contribution is as follows:

- £20,064 towards developing a new 3G artificial grass pitch to serve the locality
- £23,494 for improvements towards natural grass pitches
- £104,077 for swimming facilities,
- £94,713 towards sports hall improvements, and
- £3,682 for improvements to indoor bowling facilities.

Informal open space

SPG10 Provision of Open Space in New Housing Developments is relevant, along with policy H10 of the NLLP. For a development of this size, 10sqm per dwelling of informal open space is required on site. Based on 2,900sqm open space being provided on the site, the council would request a contribution of £80,281.20 to cover the maintenance of the land for a period of 10 years. Due to the submitted viability, it is assumed that the on-site open space is to be transferred into and maintained by a private management company. This option would mean no financial contribution is payable to the council, but should be secured within the S106 to remain perpetuity.

Area of play

SPG10 Provision of Open Space in New Housing Developments and policy R2 is relevant, along with policy H10 of the NLLP. Policy R2 of the NLLP states one LEAP should be provided for developments between 51 and 100 dwellings, and thereafter one LEAP to be provided per 100 dwellings. In this case the council are requesting an off-site area of play contribution towards Woodbine Park, Brigg. The off-site contribution will amount to £162,318 towards Woodbine Park. Due to the submitted viability appraisal, the council would be willing to accept an off-site contribution in lieu of the proposed on-site LEAP. Since the applicant has proposed an area of play on site, the council wishes to propose that the money reserved to purchase the play equipment is instead provided as an off-site contribution for the council to spend towards the enhancement and improvement of Woodbine Park, directly west of the development site.

Biodiversity

A biodiversity net gain must be secured on site; if this cannot be done through conditions on site, it may need to be included in a section 106 agreement. BNG can be provided on site and therefore no contribution is required.

Health

Policy CS24 of the CS relates to provision of health care services. The methodology used to determine a suggested amount of S106 funding is used from the Local Plan Housing and Primary Care Analysis, which uses a rate of £864.03 per market dwelling. A health contribution of £864.03 per market dwelling is required.

Public transport

Policy CS25 relates to promoting sustainable public transport. To provide additional public transport facilities to mitigate impacts arising from the development, a transport contribution of £30,000 is requested towards the development of two new hard-standing bus stops and shelters on the A18, which will serve the development with existing bus services.

Viability

Paragraph 57 of the NPPF states, 'It is up to the applicant to demonstrate whether particular circumstances justify the need for a viability assessment at the application stage. The weight to be given to a viability assessment is a matter for the decision maker, having regard to all the circumstances in the case.' Within policy CS27 (Planning obligations), the supporting text for this policy states, 'It is important that development costs, including the cost of implementing planning obligation agreements, should not prejudice development that supports North Lincolnshire Council's aspiration to see the regeneration, improvement and revitalisation of the area. If it is claimed that a development is unable to support the costs of a planning obligation (other than those essential to allow the development to proceed) then this could be the subject of negotiations. In such cases, the developer will have to demonstrate non-viability via an 'open book' approach.'

The applicant has concerns that the S106 contributions required would make the scheme unviable. As a result the applicant has submitted a viability report. This has been independently assessed by a professional expert in this field who has concluded that the scheme is unable to support any affordable housing or any financial gain contributions. Furthermore, the assessor has stated that, 'Even, if all of the S106 obligations are removed (including affordable housing), the scheme remains unviable (generating a negative land value) based on a current day basis.'

The assessor has highlighted that the development is subject to a number of technical challenges and as a result is subject to a range of abnormal costs, and that the current costs are subject to a much higher level of uncertainty than would normally be the case, due to continuing inflation. The assessor has adopted a benchmark land value of £2,325,000 and applied a profit which equates to 18% of GDV.

The assessor has also looked at the possibility that the developer may take a long-term view in the hope that when accounting for growth in the sales values and value engineering, to achieve savings in the construction costs, the scheme could produce a positive outturn. The assessor has therefore assessed the impact of changes in both the construction costs and sales values which demonstrates that there are multiple scenarios in which all or part of the current S106 package could be viable. The assessor also considered the impact of abnormal costs on the viability of the scheme and a negative land value is still generated (on a current day basis) if all of the abnormal costs are removed in their entirety.

Solely for planning purposes (as for viability testing a sensitivity exercise is not required) the purpose of the sensitivity exercise was to demonstrate the potential for the scheme to be

viable in the longer term and justify the developer's proposals for the site. The assessor advises that the council may wish to consider 'some form of clawback arrangement within the revised S106 Agreement whereby a further open book appraisal (i.e. late stage appraisal) will need to be completed after the scheme has been completed and if the clawback triggers are exceeded a financial contribution in lieu of affordable housing and current financial contributions become payable to the council.'

Although the site is not viable for any affordable housing and financial contributions, a S106 will still be required to secure the open space on the site and its maintenance (by a management company). The developer has also agreed to remove the LEAP on site and make a financial contribution of £60,000 towards the nearby Woodbine Park to improve the existing play facilities on/in this park.

A 'clawback' mechanism is also recommended within the S106 to ensure if the scheme does become viable once the development is completed/partially completed, then the council can seek S106 contributions from the developer to mitigate the impacts of the development in accordance with the planning policies mentioned above.

It is acknowledged that the development will only result in contributions for open space and an off-site financial contribution to improve the existing LEAP at Woodbine Park to mitigate the impacts of the development. The site has been allocated for housing since 2016 and has not seen delivery. The provision of the proposed 290 homes on this allocated housing site, would provide a very high number of dwellings, contributing positively to the council's housing land supply and providing a mix of housing types to meet the needs of the residents of North Lincolnshire. The new dwellings would support the existing services and facilities in the market town of Brigg and also those in Wrawby. The development proposed is a high quality development appropriate to its context. The development will deliver significant infrastructure benefits to Brigg in that the first part of the Brigg Link Road will be constructed as part of the development proposal. Therefore, on balance, the removal of the S106 contributions, apart from the provision of open space and maintenance on the site, and off-site financial contribution towards Woodbine Park, together with the 'clawback' mechanism to allow the council to seek S106 contributions potentially in the future if the scheme does prove to be viable, will allow for the delivery of this residential scheme on this allocated housing site which will provide significant overarching benefits to Brigg and Wrawby and to North Lincolnshire as a whole. The long-term future of the site for residential development will be secured. The developer is a national housebuilder who has shown commitment to the site and is also the applicant for the adjacent site under pending outline application PA/2023/1425. The scheme therefore complies with the relevant policies in the NPPF and Core Strategy, which confirm that there can be flexibility with regard to obligations that would make a scheme financially unviable.

Other matters

Objections from Brigg Town Council and Wrawby Parish Council, and objectors, are noted. These objections have been largely addressed in this report. There is no evidence to suggest that the infrastructure and local services are unable to cope with the development, and there are no objections from statutory consultees to the development. The site will result in the loss of a greenfield site but the site is allocated for residential development in the development plan. The site is close to public transport links. Noise and vibration impacts will largely be during the construction phase and mitigated to a certain extent by planning conditions. There will be an increase in vehicles and pedestrians using Wrawby Road (A18) but this is part of the strategic road network and once the whole link road is

completed the site will be linked to Atherton Way. There will be no affordable housing on the site or other financial contributions apart from an off-site contribution to Woodbine Park. This has been justified in the submitted viability assessment which has been appraised by an independent expert in the field. The poplar trees to the western boundary are to be retained. There is no evidence to suggest wildlife will be lost on the site. The significant landscaping proposed and open space and biodiversity enhancements will provide wildlife gains on the site. The site is not over developed and is in character with this area of Brigg. The developer has carried out public engagement, and site and press notices have advertised the proposals accordingly. The application has also been placed on the council's website with all the statutory consultations carried out. Bungalows are provided on the site and, due to viability issues already on the site, it is accepted that further bungalows cannot be achieved on the site. There are no objections to the drainage proposals for the site. The site is also at low risk of flooding, being within flood zone 1. The development will not result in loss of privacy to occupiers of the development or existing occupiers. This has been assessed more fully in earlier sections of this report. The main access for this development at this site is from Wrawby Road.

Planning balance and conclusion

This full application seeks planning permission for a residential development comprising 290 dwellings with associated works, including highways, open space, landscaping and drainage infrastructure. The proposal will also provide significant infrastructure by the construction of the first part of the Brigg Link Road from Wrawby Road into the site.

The proposal does involve the loss of some trees, but landscaping conditions can be used to secure a robust landscaping scheme on the site based on the submitted landscape master plan. The development will be visible on the landscape as the site will change from agricultural land to residential development. However, once the landscaping has matured over several years, the site will be assimilated into the landscape. BNG can be provided on the site due to the significant open space and landscaping proposed.

The site is allocated for residential development as part of the adopted development plan and its development would make a significant contribution towards meeting housing delivery targets. A mix of housing types and designs are proposed which will provide housing for the residents of North Lincolnshire. Dwellings will be constructed using modern sustainable building techniques. The site is in a sustainable location and will support the existing facilities and services in Brigg improving the sustainability of the site further. Occupiers of the development will also support the existing services and facilities within Brigg, improving the vitality and viability of the town. As outlined above, there will be no unacceptable impacts with regard to highway safety, amenity, drainage, ecology, contamination or noise, subject to planning conditions to mitigate any impacts identified. Furthermore, there have been no objections from any statutory consultees to the proposed development.

Therefore, subject to the recommended conditions and a section 106 agreement to secure public open space and maintenance on the site, an off-site contribution toward play facilities at Woodbine Park and a clawback mechanism whereby a further open book appraisal (i.e. late stage appraisal) will need to be completed after the scheme has been completed, and if the clawback triggers are exceeded a financial contribution in lieu of affordable housing and current financial contributions becomes payable to the council, it is considered the proposal is acceptable and this application should be supported.

Pre-commencement conditions

All pre-commencement conditions have been agreed with the developer.

RECOMMENDATION

Subject to the completion of a formal agreement under Section 106 of the Town and Country Planning Act 1990 (or other appropriate legislation) providing for the provisions and maintenance of open space within the development, an off-site financial contribution of £60,000 towards play equipment at Woodbine Park and a clawback mechanism whereby a further open book appraisal (i.e. late stage appraisal) will need to be completed after the scheme has been completed, and if the clawback triggers are exceeded a financial contribution in lieu of affordable housing and current financial contributions becomes payable to the council, the committee resolves:

- (i) it is mindful to grant permission for the development;
- (ii) the decision be delegated to the Development Management Lead upon completion of the obligation;
- (iii) if the obligation is not completed by 17 October 2024 in accordance with the above recommendation, the Development Management Lead be authorised to refuse the application on grounds of lack of open space on the site and lack of an off-site financial contribution towards improving the existing Woodbine Park to mitigate impacts of the development;
- (iv) the permission so granted be subject to the following conditions:

1.

The development must be begun before the expiration of three years from the date of this permission.

Reason

To comply with section 91 of the Town and Country Planning Act 1990.

2.

The development hereby permitted shall be carried out in accordance with the following approved schedule and plans:

Brigg Plans Schedule dated 26 March 2024

Drawing no: 2250.B.04 0.9m Estate Railings

Drawing no: 1160-009 Rev A Duel Pumping Station Layout

Drawing no: 160-006 Rev A Proposed Levels Strategy Sheet 2

Drawing no: 160-005 Rev A Proposed Levels Strategy Sheet 1

Drawing No: 2250 01 R - Trees to be Retained.

Reason

For the avoidance of doubt and in the interests of proper planning.

3.

No development shall commence on site until a phasing plan setting out the proposed phasing of the construction of the development has been submitted to and approved in writing by the local planning authority. The development shall thereafter be carried out in accordance with the approved phasing plan.

Reason

To ensure the development is brought forward in a coherent and planned manner.

4.

No dwelling on the site shall be occupied until the vehicular access to it and the vehicle parking facilities serving it have been completed and, once provided, the vehicle parking spaces shall be retained.

Reason

In the interests of highway safety and to comply with policies T2 and T19 of the North Lincolnshire Local Plan.

5.

No dwelling on the site shall be occupied until improvements to the footway/cycleway across the site frontage have been completed in accordance with details to be submitted to and approved in writing by the local planning authority.

Reason

In the interests of highway safety and to ensure pedestrians and cycle linkages are provided along Wrawby Road in accordance with policies T2, T6 and T8 of the North Lincolnshire Local Plan and policy CS5 of the Core Strategy.

6.

No loose material shall be placed on any driveway or parking area within 10 metres of the adopted highway unless measures are taken to the satisfaction of the local planning authority to prevent the material from spilling onto the highway. Once agreed and implemented these measures shall be retained.

Reason

In the interests of highway safety and to comply with policy T19 of the North Lincolnshire Local Plan.

7.

No dwelling served by a private driveway shall be occupied until it has been constructed in accordance with details including:

- the proposed method of forming access from the highway, including the required visibility splays;
- the method of constructing/paving the drive;
- the provision of adequate drainage features;
- the provision of suitable bin collection facilities adjacent to the highway;

- the provision of suitable lighting arrangements;
- the provision of street name plates that shall include the words 'Private Drive';

which have been agreed in writing by the local planning authority. Once constructed the private driveway shall be retained.

Reason

In the interests of highway safety and to comply with policies T2 and T19 of the North Lincolnshire Local Plan.

8.

Works shall not commence on site until wheel-cleaning facilities, in accordance with details to be approved in writing by the local planning authority, have been provided within the curtilage of the site and this facility shall be retained for the duration of the works.

Reason

To prevent material being deposited on the highway and creating unsafe road conditions.

9.

No above-ground works shall commence until the details of the pedestrian/cycle linkages onto South View Avenue and Wrawby Road, including the means of preventing unauthorised access by vehicles, have been submitted to and approved in writing by the local planning authority. Thereafter, the approved works shall be implemented as approved and in accordance with the Phasing Plan approved under condition 3.

Reason

To ensure satisfactory pedestrian and cycling linkages from the site to improve sustainability in accordance with policies T6 and T8 of the North Lincolnshire Local Plan and policy CS25 of the Core Strategy.

10.

No above-ground works shall commence until details of the cycle parking for each dwelling have been submitted to and approved in writing by the local planning authority. The cycle parking provision shall be secure and covered as a minimum. No dwelling shall be occupied until the cycle parking has been provided in accordance with the approved details.

Reason

To ensure satisfactory cycle parking provision on the site in accordance with policy T8 of the North Lincolnshire Local Plan and policies CS5 and CS25 of the Core Strategy.

11.

No above-ground works shall commence until details of a suitable, controlled pedestrian/cycle crossing of the Brigg Link Road, in a location to be agreed and including timescales for construction and completion, have been submitted to and approved in writing by the local planning authority. Thereafter, only the approved scheme shall be installed on the site and retained.

Reason

In the interests of highway safety.

12.

No development shall begin on site until details of:

- the method of providing the new vehicular access point onto Wrawby Road, including required visibility splays;
- the layout, drainage, construction, services and lighting of the proposed access roads;
- the proposed pedestrian and cycle infrastructure within the site, including the priority crossing facility at the site access with Wrawby Road;

have been submitted to and approved in writing by the local planning authority.

Reason

In the interests of highway safety and to comply with policies T2, T6, T8 and T19 of the North Lincolnshire Local Plan and policy CS25 of the Core Strategy.

13.

No dwelling on the site shall be occupied until the access road and footway has been completed to at least base course level and adequately lit from the junction with the adjacent highway up to the access to the dwelling.

Reason

In the interests of highway safety and to comply with policies T2 and T19 of the North Lincolnshire Local Plan.

14.

The penultimate dwelling on site on any phase/sub-phase shall not be occupied until the access roads have been completed for that phase/sub-phase in accordance with the approved phasing plan (condition 3).

Reason

In the interests of highway safety and to comply with policies T2 and T19 of the North Lincolnshire Local Plan.

15.

No development, whether permitted by the Town and Country Planning (General Permitted Development) Order or not, shall take place within any service strip adjacent to any shared surface road, and any planting or landscaping within this service strip shall be of species which shall be agreed in writing with the local planning authority prior to planting.

Reason

In the interests of highway safety and to comply with policies T2 and T19 of the North Lincolnshire Local Plan.

16.

Within six months of the occupation of the first dwelling on site, the residential travel plan shall be submitted to and approved in writing by the local planning authority. The residential travel plan shall include details of:

- a residential travel plan co-ordinator, including name, contact details, job description, weekly hours and annual budget for promotion, publicity and monitoring;
- details of how sustainable transport, such as walking, cycling and public transport will be promoted;
- a monitoring strategy.

Once approved, the plan shall be implemented, reviewed, updated and amended as necessary for five years from the date of first occupation.

Reason

To ensure sustainable transport for the site in perpetuity in accordance with policy CS25 of the Core Strategy.

17.

Unless otherwise agreed in writing by the local planning authority in consultation with National Highways (or its successors), no construction shall commence unless and until a detailed construction phase traffic management plan has been submitted to and approved in writing. Thereafter, the construction shall be undertaken in accordance with the approved plan.

Reason

In the interests of highway safety and to comply with policy T19 of the North Lincolnshire Local Plan and to safeguard the operation and free flow of traffic on the M180 strategic road network.

18.

A boundary treatment plan shall be submitted to and approved in writing by the local planning authority (in consultation with National Highways) for the M180 before commencement of development. The boundary treatment plan shall include as a minimum:

- (i) details of the fencing location, type, construction method and maintenance;
- (ii) details for management of existing boundary planting to include an arboricultural tree survey and tree protection plan with a method statement for any works required to address the removal, retention and management of trees along this boundary.

All works shall be undertaken in accordance with the agreed plan prior to commencement of development and maintained in perpetuity as such thereafter.

Reason

For reasons of safety, liability and maintenance in accordance with paragraph 57 of Department of Transport Circular 01/2022.

19.

Prior to construction above damp-proof course, a phasing plan setting out the details of the phasing of the development for foul water drainage shall be submitted to and approved in writing by the local planning authority. Thereafter, the foul water drainage development shall be carried out in complete accordance with the approved phasing plan.

Reason

To ensure the development is phased to avoid an adverse impact on drainage infrastructure in accordance with policies CS18 and CS19 of the Core Strategy.

20.

Prior to the construction above damp-proof course, a scheme for on-site foul water drainage works, including connection point and discharge rate, shall be submitted to and approved in writing by the local planning authority. Prior to the occupation of any dwelling for any phase of the development, the foul water drainage works relating to that phase must have been carried out in complete accordance with the approved scheme.

Reason

To prevent environmental and amenity problems arising from flooding in accordance with policies CS18 and CS19 of the Core Strategy, and policy DS14 of the North Lincolnshire Local Plan.

21.

No development shall commence until details of a method of preventing surface water run-off either from hard-paved areas within the site onto the highway or from the highway onto the site have been submitted to and approved in writing by the local planning authority. These facilities shall be implemented prior to the access and parking facilities being brought into use and thereafter so retained.

Reason

In the interests of highway safety.

22.

No development shall take place until a scheme for surface water disposal, including the maintenance and adoption arrangements, for each phase of the development, in accordance with the approved phasing plan (condition 3), has been submitted to and agreed in writing by the local planning authority. This must be based upon the submitted Flood Risk Assessment dated March 2024 by Fortem Civil Engineering Consultants Ltd and appendices A, B, C and D, drawing no 1160-SK05 Surface Water Drainage Treatment Review, drawing no 1160-Sk04 Rev A Surface Water Drainage Principles Review (LLFA) and drawing no 1160-003 Rev D Proposed Drainage Strategy, SuDS Design Statement report dated March 2024, SuDS Design Summary dated 06/03/2024, SuDS Design Report Appendix A and B. No dwelling shall be occupied in any phase of the development until it is connected to the approved drainage system, which shall thereafter be retained and maintained in accordance with the approved details.

Reason

To prevent the increased risk of flooding to themselves and others, to improve and protect water quality, and to ensure the implementation and future maintenance of the sustainable drainage structures in accordance with policy DS16 of the North Lincolnshire Local Plan, policies CS18 and CS19 of the North Lincolnshire Core Strategy, and paragraphs 165 to 175 of the National Planning Policy Framework.

23.

No development shall take place until a noise mitigation scheme has been submitted to and approved in writing by the local planning authority. As a minimum this noise mitigation scheme shall include details of:

- noise mitigation measures;
- predicted noise levels to be achieved at sensitive locations as a result of the noise mitigation scheme;
- how the noise mitigation scheme will be maintained for the lifetime of the development.

The noise mitigation scheme shall be implemented before occupation of the development and shall be retained thereafter.

Reason

To protect the amenity of the residents of the proposed dwellings in accordance with policy CS5 of the Core Strategy, and policies DS1 and DS11 of the North Lincolnshire Local Plan.

24.

Prior to the occupation of the dwellings hereby permitted and following installation of the mitigation measures in accordance with the approved technical specification required by condition 23 above, a verification report that demonstrates the effectiveness of the mitigation measures shall be undertaken. The verification report shall be submitted to and approved in writing by the local planning authority.

Reason

To protect the amenity of the residents of the proposed dwellings in accordance with policy CS5 of the Core Strategy, and policies DS1 and DS11 of the North Lincolnshire Local Plan.

25.

Unless otherwise agreed by the local planning authority, development other than that required to be carried out as part of an approved scheme of remediation must not commence until parts 1 to 4 below have been complied with. If unexpected contamination is found after development has begun, development must be halted on that part of the site affected by the unexpected contamination to the extent specified by the local planning authority in writing until part 4 has been complied with in relation to that contamination.

Part 1: Site Characteristics

A Phase 1 desk study shall be carried out to identify and evaluate all potential sources of contamination and the impacts on land and/or controlled waters, relevant to the site. The desk study shall establish a 'conceptual model' of the site and identify all plausible pollutant linkages. Furthermore, the assessment shall set objectives for intrusive site investigation works/Quantitative Risk Assessment (or state if none required). Two full copies of the desk study and a non-technical summary shall be submitted to the local planning authority for approval prior to proceeding to further site investigation.

An investigation and risk assessment, in addition to any assessment provided with the planning application, must be completed in accordance with a scheme to assess the nature and extent of any contamination on the site, whether or not it originates on the site. The contents of the scheme are subject to the approval in writing of the local planning authority. The investigation and risk assessment must be undertaken by competent persons and a written report of the findings must be produced. The written report is subject to the approval in writing of the local planning authority. The report of the findings must include:

- (i) a survey of the extent, scale and nature of contamination;
- (ii) an assessment of the potential risks to:
 - human health;
 - property (existing or proposed) including buildings, crops, livestock, pets, woodland, and service lines and pipes;
 - adjoining land;
 - groundwaters and surface waters;
 - ecological systems;
 - archaeological sites and ancient monuments;
- (iii) an appraisal of remedial options, and a proposal of the preferred option(s).

This must be conducted in accordance with Environment Agency's Land Contamination Risk Management (LCRM) guidance April 2021.

Part 2: Submission of Remediation Scheme

A detailed remediation scheme to bring the site to a condition suitable for the intended use by removing unacceptable risks to human health, buildings and other property and the natural and historical environment must be prepared, and is subject to the approval in writing of the local planning authority. The scheme must include all works to be undertaken, proposed remediation objectives and remediation criteria, timetable of works and site management procedures. The scheme must ensure that the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.

Part 3: Implementation of Approved Remediation Scheme

The approved remediation scheme must be carried out in accordance with its terms prior to the commencement of development other than that required to carry out remediation, unless otherwise agreed in writing by the local planning authority. The local planning authority must be given two weeks' written notification of commencement of the remediation scheme works.

Following completion of measures identified in the approved remediation scheme, a verification report (referred to in PPS23 as a validation report) that demonstrates the effectiveness of the remediation carried out must be produced, and is subject to the approval in writing of the local planning authority.

Part 4: Reporting of Unexpected Contamination

In the event that contamination is found at any time when carrying out the approved development that was not previously identified it must be reported in writing immediately to the local planning authority. An investigation and risk assessment must be undertaken in accordance with the requirements of Part 1, and where remediation is necessary a remediation scheme must be prepared in accordance with the requirements of Part 2, which is subject to the approval in writing of the local planning authority.

Following completion of measures identified in the approved remediation scheme a verification report must be prepared, which is subject to the approval in writing of the local planning authority in accordance with Part 3.

Reason

To ensure the site is safe for future users and construction workers in accordance with policies DS1 and DS7 of the North Lincolnshire Local Plan.

26.

Construction, demolition and site clearance operations shall be limited to the following days and hours:

- 8am to 6pm Monday to Friday

- 8am to 1pm on Saturdays.

No construction, demolition or site clearance operations shall take place on Sundays, or public and bank holidays.

HGV movements shall not be permitted outside these hours during the construction phase without prior written approval from the local planning authority.

Installation of equipment on site shall not be permitted outside these hours without prior written approval from the local planning authority.

Reason

To protect residential amenity in accordance with policy DS1 of the North Lincolnshire Local Plan.

27.

No stage of the development hereby permitted shall commence until a construction environmental management plan (CEMP) has been submitted to and approved in writing by the local planning authority. The CEMP shall include the following, and all recommendations made in Table 20 of report reference 5817r3, as a minimum:

- (i) Noise and vibration – the CEMP shall set out the particulars of:
 - (a) the works, and the method by which they are to be carried out;
 - (b) the noise and vibration attenuation measures to be taken to minimise noise and vibration resulting from the works, including any noise limits; and
 - (c) a scheme for monitoring the noise and vibration during the works to ensure compliance with the noise limits and the effectiveness of the attenuation measures.
- (ii) Light – the CEMP shall set out the particulars of:
 - (a) specified locations for contractors' compounds and materials storage areas;

- (b) areas where lighting will be required for health and safety purposes;
 - (c) the location of potential temporary floodlights;
 - (d) the identification of sensitive receptors likely to be impacted upon by light nuisance;
 - (e) proposed methods of mitigation against potential light nuisance, including potential glare and light spill, on sensitive receptors.
- (iii) Dust – the CEMP shall set out the particulars of:
- (a) site dust monitoring, recording and complaint investigation procedures;
 - (b) the identification of receptors and the related risk of dust impact at all phases of the development, including when buildings and properties start to be occupied;
 - (c) the provision of water to the site;
 - (d) dust mitigation techniques at all stages of development;
 - (e) the prevention of dust trackout;
 - (f) communication with residents and other receptors;
 - (g) a commitment to cease the relevant operation if dust emissions are identified, either by regular site monitoring or by the local authority;
 - (h) a ‘no burning of waste’ policy.

Reason

To protect residential amenity in accordance with policy DS1 of the North Lincolnshire Local Plan.

28.

No development shall take place until a species protection plan has been submitted to and approved in writing by the local planning authority. The plan shall include details of measures to avoid harm to badgers, hedgehogs and nesting birds during vegetation clearance and construction works.

Reason

To conserve protected species in accordance with saved policy LC5 of the North Lincolnshire Local Plan and policy CS17 of the Core Strategy.

29.

Within six months of the commencement of development, the applicant or their successor in title shall submit a biodiversity management plan to the local planning authority for approval in writing. The plan shall include:

- (a) details of bat boxes and bat bricks, of at least two different specifications, to be installed on 15% of dwellings;
- (b) details of swift boxes and sparrow terraces to be installed on 15% of dwellings combined;
- (c) details of nesting sites to be installed to support a variety of other species, including house martin and garden birds;
- (d) restrictions on lighting to avoid impacts on bat roosts, bat foraging areas, bird nesting sites and sensitive habitats;
- (e) provision for hedgehogs to pass through any fencing installed between gardens and between areas of grassland;
- (f) prescriptions for the retention, planting and aftercare of native trees, shrubs, hedgerows and wildflowers of high biodiversity value;
- (g) details of wetland habitat to be created and managed as part of sustainable drainage;
- (h) appropriate phasing mechanisms to ensure that significant areas of habitat are created and are becoming properly established before each phase of construction;
- (i) procedures for monitoring and ongoing management of created habitats for a period of at least 30 years;
- (j) measures to avoid light, noise and water pollution, fly-tipping and other factors that would harm habitats and species;
- (k) prescriptions for the provision of a wildlife gardening leaflet for each new resident;
- (l) details to confirm that the measures proposed will provide a measurable net gain in biodiversity value of least 1% in accordance with the Defra biodiversity metric 3.0;
- (m) proposed timings for the above works in relation to the completion of the dwellings.

Reason

To conserve and enhance biodiversity in accordance with policies CS5 and CS17 of the Core Strategy.

30.

The species protection plan and biodiversity management plan shall be carried out in accordance with the approved details and timings, and the approved features shall be retained thereafter, for a period of at least 30 years, unless otherwise approved in writing by the local planning authority. Prior to the occupation of the 50th, 100th, 150th, 200th and 250th dwelling hereby approved, the applicant or their successor in title shall submit a report to the local planning authority providing evidence of compliance with the species protection plan and biodiversity management plan.

Reason

To conserve and enhance biodiversity in accordance with policies CS5 and CS17 of the Core Strategy.

31.

Before development is commenced, details of the method of protecting the existing trees on the site shown to be retained on drawing no 2250.02 REV S (Planning Layout) throughout the construction period shall be submitted to and approved in writing by the local planning authority, and such works as may be so approved shall be carried out before development is commenced, and maintained until completion of the development. None of the trees so protected shall be wilfully damaged or destroyed, uprooted, felled, lopped or topped, nor any other works carried out which would cause damage to the root systems or otherwise threaten the lives of the trees during the period of construction without the previous written consent of the local planning authority. Any trees removed without such consent or dying or being severely damaged or becoming seriously diseased during that period shall be replaced with trees of such size and species as may be agreed with the local planning authority.

Reason

To safeguard existing trees on the site in accordance with policy LC12 of the North Lincolnshire Local Plan and policy CS5 of the Core Strategy.

32.

No works above ground level shall take place until proposals for landscaping have been submitted to and approved by the local planning authority. This landscaping scheme must be based on the submitted landscape masterplan GL187012C. The proposals shall include indications of all existing trees and hedgerows on the site, and details of any to be retained, together with measures for their protection during the course of development.

Reason

To ensure landscaping is provided on the site, and in the interests of amenity and biodiversity in accordance with policy CS5 of the Core Strategy, and BIRH-3 and BRIH-4 of the Housing and Employment Land Allocations Development Plan Document.

33.

All the approved landscaping on any approved phase of the development shall be carried out within two years of development being commenced on that phase (unless a longer period is agreed in writing by the local planning authority). Any trees or plants which die, are removed or become seriously damaged or diseased within five years from the date of planting shall be replaced in the next planting season with others of similar size and species to those originally required to be planted, unless the local planning authority agrees in writing to any variation.

Reason

To ensure landscaping is provided on the site, and in the interests of amenity and biodiversity in accordance with policy CS5 of the Core Strategy, and BIRH-3 and BIRH-4 of the Housing and Employment Land Allocations Development Plan Document.

34.

Before any dwelling is first occupied, any WC, en-suite and bathroom windows installed in the dwelling shall be obscure glazed to a minimum of Privacy Level 3 in accordance with the Pilkington Scale of Obscuration and retained in that condition thereafter.

Reason

To safeguard privacy for occupants of the proposed dwellings.

Informatives

1.

This application must be read in conjunction with the relevant Section 106 Agreement.

2.

The development hereby granted planning permission requires works to be carried out within the limits of the adopted (public) highway. Therefore:

- before ANY construction works take place within the limits of the highway you MUST contact the highway authority on telephone number 01724 297000 to arrange for the relevant permissions/licenses to be issued;
- before ANY service (utility) connections take place within the limits of the highway you MUST contact the highway authority on telephone number 01724 297319 to arrange for the relevant permissions/licenses to be issued.

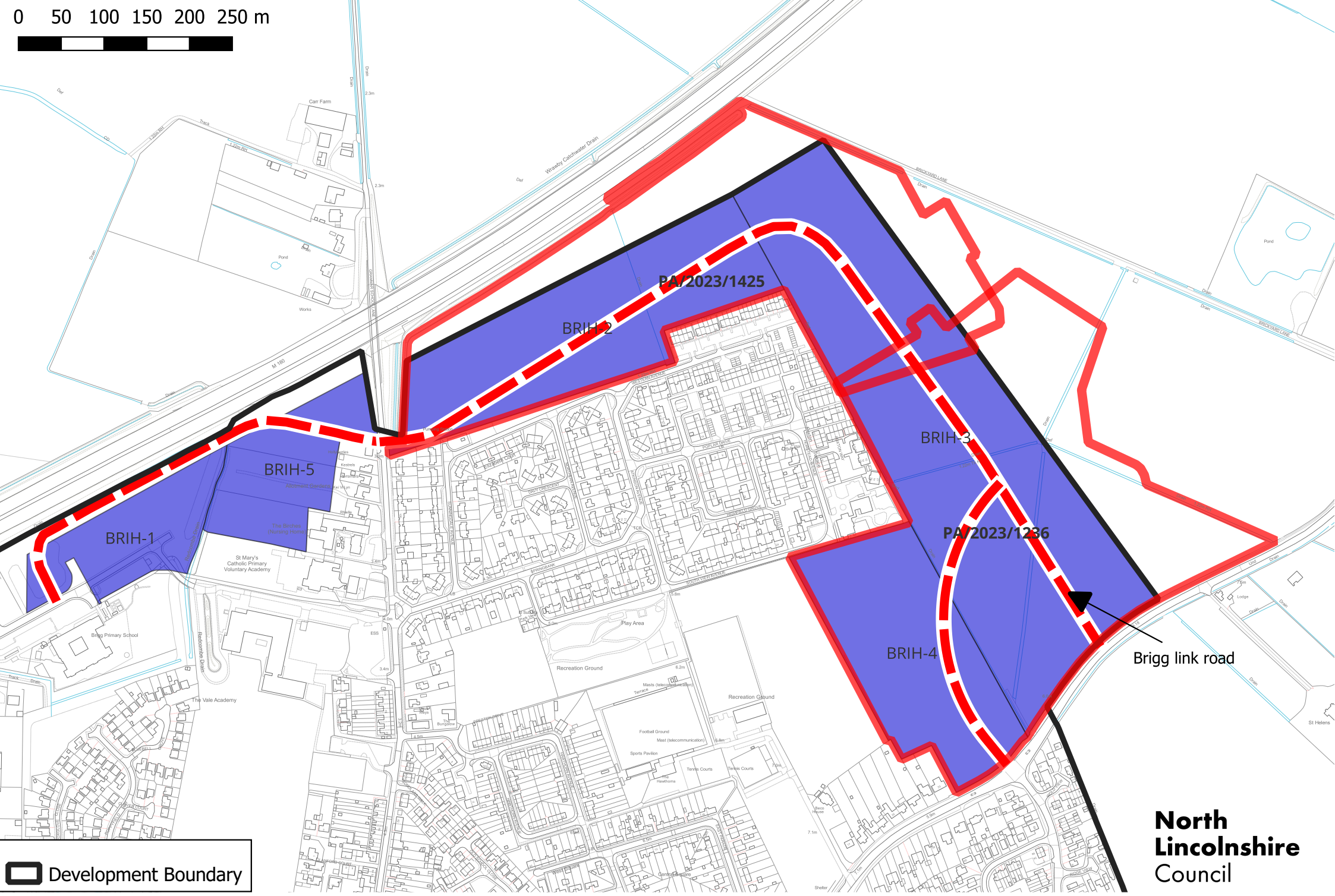
3.

In determining this application, the council, as local planning authority, has taken account of the guidance in paragraph 38 of the National Planning Policy Framework in order to seek to secure sustainable development that improves the economic, social and environmental conditions of the area.

4.

The applicant's attention is drawn to the comments made by Anglian Water and the Shire Group IDB.

0 50 100 150 200 250 m



 Development Boundary

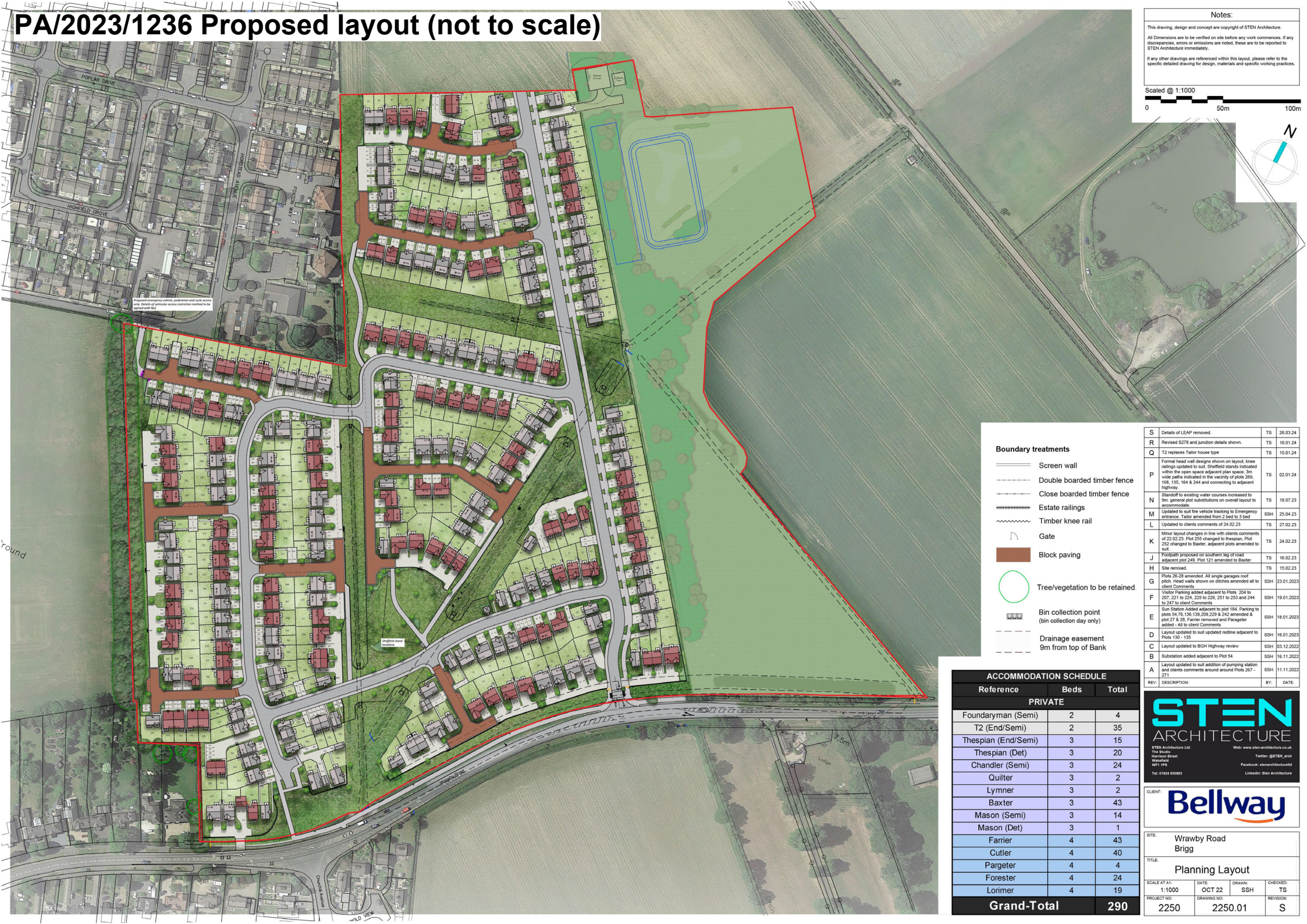
PA/2023/1236

**North
Lincolnshire
Council**

PA/2023/1236 Proposed layout (not to scale)

Notes:
 This drawing, design and concept are copyright of STEN Architecture.
 All Dimensions are to be verified on site before any work commences. If any discrepancies, errors or omissions are noted, these are to be reported to STEN Architecture immediately.
 If any other drawings are referenced within this layout, please refer to the specific detailed drawing for design, materials and specific working practices.

Scaled @ 1:1000
 0 50m 100m



- Boundary treatments**
- Screen wall
 - Double boarded timber fence
 - Close boarded timber fence
 - Estate railings
 - Timber knee rail
 - Gate
 - Block paving
 - Tree/vegetation to be retained.
 - Bin collection point (bin collection day only)
 - Drainage easement 9m from top of Bank

S	Details of LEAP removed	TS	26.03.24
R	Revised S279 and junction details shown.	TS	18.01.24
Q	T2 replaces Tailor house type	TS	15.01.24
P	Formal head wall designs shown on layout, knee railings updated to suit. Sheffield stands indicated within the open space adjacent plot spaces. 3m wide paths indicated in the vicinity of plots 269, 105, 135, 164 & 244 and connecting to adjacent highways.	TS	02.01.24
N	Standoff to existing water courses increased to 5m, general plot substitutions on overall layout to accommodate.	TS	18.07.23
M	Updated to suit fire vehicle tracking to Emergency entrance. Tailor amended from 2 beds to 3 beds	SSH	25.04.23
L	Updated to clients comments of 24.02.23	TS	27.02.23
K	Minor layout changes in line with clients comments of 22.02.23. Plot 255 changed to thespian, Plot 255 changed to Baxter, adjacent plots amended to suit.	TS	24.02.23
J	Footpaths proposed on southern leg of road adjacent plot 249. Plot 121 amended to Baxter	TS	16.02.23
H	Site removed.	TS	15.02.23
G	Plots 26-28 amended. All single garages roof pitch, head walls shown on dishes amended all to client Comments	SSH	23.01.2023
F	Visitor Parking added adjacent to Plots 238 to 247, 221 to 224, 225 to 228, 251 to 253 and 244 to 247 to client Comments	SSH	19.01.2023
E	San Station added adjacent to plot 184. Parking to plots 54, 76, 136, 138, 209, 229 & 242 amended & plot 27 & 28. Farrier removed and Paragater added - All to client Comments	SSH	18.01.2023
D	Layout updated to suit updated redline adjacent to Plots 150 - 155	SSH	16.01.2023
C	Layout updated to BCH Highway review	SSH	03.12.2022
B	Substation added adjacent to Plot 54	SSH	16.11.2022
A	Layout updated to suit addition of pumping station and clients comments around around Plots 267 - 271	SSH	11.11.2022
REV	DESCRIPTION:	BY:	DATE:

ACCOMMODATION SCHEDULE		
Reference	Beds	Total
PRIVATE		
Foundryman (Semi)	2	4
T2 (End/Semi)	2	35
Thespian (End/Semi)	3	15
Thespian (Det)	3	20
Chandler (Semi)	3	24
Quilter	3	2
Lymner	3	2
Baxter	3	43
Mason (Semi)	3	14
Mason (Det)	3	1
Farrier	4	43
Cutler	4	40
Pargeter	4	4
Forester	4	24
Lorimer	4	19
Grand-Total		290

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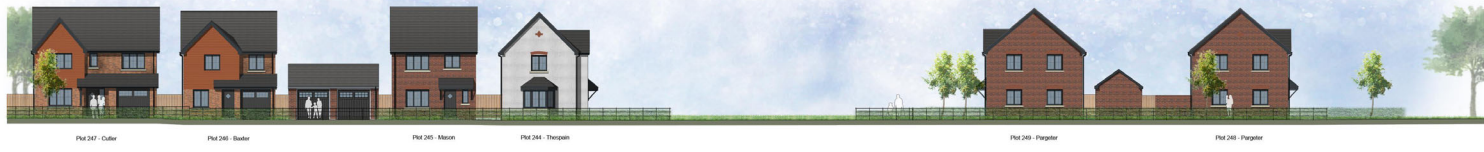
CLIENT: **Bellway**

SITE: **Wrawby Road
Brigg**

TITLE: **Planning Layout**

SCALE AT A1:	DATE:	DRAWN:	CHECKED:
1:1000	OCT 22	SSH	TS
PROJECT NO:	DRAWING NO:	REVISION:	
2250	2250.01	S	

PA/2023/1236 Street scenes A-A, B-B and C-C (not to scale)



WRAWBY ROAD STREET SCENE A-A @ 1:200



WRAWBY ROAD STREET SCENE B-B @ 1:200



WRAWBY ROAD STREET SCENE B-B CONTINUED @ 1:200



STREET SCENE C-C @ 1:200



STREET SCENE C-C CONTINUED @ 1:200



KEY NTS

Levels shown on street Scenes are indicative. Please refer to engineering details for further clarification.

E	Updated to Planning Layout revision D.	TS	26.03.24
D	Street scenes A-A & B-B indicated as Wrawby Road Street Scenes as per clients comments.	LS	17.10.23
C	Updated to Planning Layout revision N. Street scenes A-A & B-B updated as per changes to house type specification.	LS	13.10.23
B	Updated to Planning Layout revision L.	VR	17.04.23
A	Updated to Planning Layout revision B. engineers levels applied to street scenes as per clients comments.	LS	22.11.22
REV:	DESCRIPTION:	BY:	DATE:

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CLIENT: **Bellway**

SITE: **Brigg**

TITLE: **Street Scenes**

SCALE AT A1:	DATE:	DRAWN:	CHECKED:
1:200	18.10.22	LS	-
PROJECT NO:	DRAWING NO:	REVISION:	
2250	2250.04.01	E	



PA/2023/1236 Street scenes D-D and E-E (not to scale)



STREET SCENE D-D @ 1:200



STREET SCENE D-D CONTINUED @ 1:200



STREET SCENE E-E @ 1:200



STREET SCENE E-E CONTINUED @ 1:200



KEY NTS

Levels shown on street Scenes are indicative.
Please refer to engineering details for further clarification.

D	Updated to Planning Layout revision B.	TS	28.03.24
C	Updated to Planning Layout revision N.	LS	13.10.23
B	Updated to Planning Layout revision L.	VR	17.04.23
A	Updated to Planning Layout revision B, engineers levels applied to street scenes as per clients comments.	LS	22.11.22
REV:	DESCRIPTION:	BY:	DATE:

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TITLE: **Street Scenes**

SCALE AT A1:	DATE:	DRAWN:	CHECKED:
1:200	18.10.22	LS	-
PROJECT NO:	DRAWING NO:	REVISION:	
2250	2250.04.02	D	

